

<b>Title</b>	<b>Planning Applications</b>
<b>To:</b>	<b>Planning Control Committee</b>
<b>On:</b>	<b>29 September 2015</b>
<b>By:</b>	<b>Development Manager</b>
<b>Status:</b>	<b>For Publication</b>

### Executive Summary

The attached reports present members with a description of various planning applications, the results of consultations, relevant policies, site history and issues involved.

My recommendations in each case are given in the attached reports.

### This report has the following implications

**Township Forum/ Ward:** Identified in each case.

**Policy:** Identified in each case.

**Resources:** Not generally applicable.

**Equality Act 2010:** All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:  
The elimination of discrimination, harassment and victimisation;  
The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;  
The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

**Human Rights:** All planning applications are considered against the provisions of the Human Rights Act 1998.

Under Article 6 the applicants (and those third parties who have made representations) have the right to a fair hearing and to this end full consideration will be given to their comments.

Article 8 and Protocol 1 of the First Article confer a right to respect private and family life and a right to the protection of property, ie peaceful enjoyment of one's possessions which could include a person's home, and other land and business assets.

In taking account of the Council policy as set out in the Bury Unitary Development Plan 1997 and all material planning considerations, I have concluded on balance that the rights conferred upon the applicant/ objectors/ residents/ other interested party by Article 8 and Article 1 of the First Protocol may be interfered with, since such interference is in accordance with the law and is justified in the public interest. Any restriction of these rights posed by refusal/ approval of the application is legitimate since it is proportionate to the wider benefits of such a decision, is based

upon the merits of the proposal, and falls within the margin of discretion afforded to the Council under the Town & Country Planning Acts.

## **Development Manager**

### **Background Documents**

1. The planning application forms and plans submitted therewith.
2. Certificates relating to the ownership.
3. Letters and Documents from objectors or other interested parties.
4. Responses from Consultees.

FOR FURTHER INFORMATION ON THE CONTENTS OF EACH REPORT PLEASE CONTACT INDIVIDUAL CASE OFFICERS IDENTIFIED IN EACH CASE.

<b>01</b>	<b>Township Forum - Ward:</b> Radcliffe - East	<b>App No.</b> 58324
<b>Location:</b>	Land adjacent to Eton Business Park, Bury Road, Radcliffe, Manchester, M26 2XF	
<b>Proposal:</b>	Residential development of 18 no. dwellings	
<b>Recommendation:</b>	Minded to Approve	<b>Site Visit:</b> N
<hr/>		
<b>02</b>	<b>Township Forum - Ward:</b> Prestwich - St Mary's	<b>App No.</b> 58655
<b>Location:</b>	Land at Valley Park Road/Clifton Road Prestwich, Manchester, M25 3TG	
<b>Proposal:</b>	Erection of 97 no. residential units (Class C3) comprising 67 no. dwellings and 30 no. apartments with associated landscaping, access arrangements and car parking and substation and creation of pond and woodland walk and seating area	
<b>Recommendation:</b>	Minded to Approve	<b>Site Visit:</b> N
<hr/>		
<b>03</b>	<b>Township Forum - Ward:</b> Bury West - Elton	<b>App No.</b> 58805
<b>Location:</b>	Land between 99 and 133 Crostons Road and Tottington Road, Bury, BL8 1AL	
<b>Proposal:</b>	Erection of 9 no. dwellings	
<b>Recommendation:</b>	Approve with Conditions	<b>Site Visit:</b> N
<hr/>		
<b>04</b>	<b>Township Forum - Ward:</b> Whitefield + Unsworth - Pilkington Park	<b>App No.</b> 58874
<b>Location:</b>	Whitefield Golf Club, Higher Lane, Whitefield, Manchester, M45 7EZ	
<b>Proposal:</b>	Erection of 1 no. 30M (hub) high, 45m (tip) high wind turbine plus ancillary development	
<b>Recommendation:</b>	Refuse	<b>Site Visit:</b> Y
<hr/>		
<b>05</b>	<b>Township Forum - Ward:</b> Whitefield + Unsworth - Besses	<b>App No.</b> 59051
<b>Location:</b>	116 Bury New Road, Whitefield, Manchester, M45 6AD	
<b>Proposal:</b>	Retrospective application for change of use of part of ground floor from offices (A2) to taxi booking office (Sui Generis)	
<b>Recommendation:</b>	Approve with Conditions	<b>Site Visit:</b> N
<hr/>		
<b>06</b>	<b>Township Forum - Ward:</b> Whitefield + Unsworth - Pilkington Park	<b>App No.</b> 59058
<b>Location:</b>	Land adjacent to 5 West Avenue, Whitefield, Manchester, M45 7SA	
<b>Proposal:</b>	Outline - Demolition of existing building and erection of 4 no. dwellings	

**Recommendation:** Approve with Conditions

**Site Visit:** N



**Ward:** Radcliffe - East

Item 01

**Applicant:** Southdale

**Location:** Land adjacent to Eton Business Park, Bury Road, Radcliffe, Manchester, M26 2XF

**Proposal:** Residential development of 18 no. dwellings

**Application Ref:** 58324/Full

**Target Date:** 21/04/2015

**Recommendation:** Minded to Approve

**It is recommended that this application is Minded to Approve subject to the signing and completion of a Section 106 agreement for recreation provision in accordance with Policy RT2/2 of the adopted Unitary Development Plan. Should the agreement not be signed and completed within a reasonable period, it is requested that the application be determined by the Development Manager.**

### **Description**

The site forms part of an employment generating area and is located close to the junction of Bury Road and Eton Hill Road. The site consists of a hard standing, with trees and vegetation on the perimeter. There is a low brick wall along the boundary with Bury Road and 2 metre high palisade fencing to all other boundaries. Access to the site is from the existing vehicular access, which is shared with the adjacent property.

There is open grazing land to the north of the site and Hutchinson's Goit forms the boundary to the east. To the south and south east are industrial buildings and residential dwellings to the west across Bury Road.

Planning permission was granted in September 2011 for the provision of 194 self-storage containers, which would be stacked at 2 storeys in height. The units would be let to commercial and members of the public for both short term and long term storage. However, this was not implemented due to viability concerns.

The proposed development involves the erection of 18 dwellings, which would all be semi-detached properties. The proposed dwellings would be two storeys in height and would be constructed from a mixture of brick with render panels and a tile roof. Vehicular access would be taken from a new access off Bury Road.

### **Relevant Planning History**

35433 - External storage of the steel work and cladding of a dismantled prefabricated building to a maximum height of 2 metres for a period of no more than 2 years at land adjacent to Remploy factory at junction of Bury Road and Eton Hill Road, Radcliffe. Approved with conditions - 29 July 1999

54131 - Erection of 194 no. self-storage container units stacked in 2 storeys at Eton Business Park, Eton Hill Road, Radcliffe. Approved with conditions - 21 September 2011.

Adjacent site

45808 - Temporary sales cabin at land at Bury Road, adjacent to Remploy Ltd, Radcliffe. Approved with conditions - 29 March 2006.

49849 - Change of use to include the reception, storage and treatment to recycle electrical and electronic equipment at remploy, Eton Hill Road, Radcliffe. Approved with conditions - 21 July 2008.

51808 - Change of use of industrial unit from electrical and electronic equipment recycling

(sui generis) to vehicle repair and MOT testing centre (Class B2) use; addition of mezzanine floor; new vehicular entrance and alterations to car park at Remploy, Eton Hill Road, Radcliffe. Approved with conditions - 16 December 2009.

52569 - Application for non-material amendment to approved application 51808 to reduce size of approved vehicle repair and mot testing scheme; deletion of exhaust/extraction flues; elevational alterations at Eton Business Park, Eton Hill Road, Radcliffe. Approved - 12 July 2010

### **Publicity**

43 neighbouring properties were notified by means of a letter on 21 January and a press notice was published in the Bury Times on 29 January 2015. Site notices were posted on 23 January 2015.

6 letters have been received from the occupiers of 271, 283 Bury Road and Eton Way North, which have raised the following issues:

- Noise pollution.
- Impact upon privacy.
- Road works outside my home and obstructions to accessing my home.
- Concerns regarding dust during construction.
- Bought a house near here because of the beautiful views across the field.
- The proposed development would impact upon wildlife.
- The proposal would lead to an increase in traffic.
- Morris Homes are developing housing 400 yards up the road. We don't need any more housing on this road.
- Will traffic calming measures be put in place?
- Object to dwellings being located in an industrial area as this would prevent expansion of much needed employment.
- There are deliveries from HGVs, which does not mix with residents.

The application was advertised as a departure by publishing a press notice in the Bury Times on 28 May 2015 and by site notices, which were posted on 22 May 2015.

No response to date. Any comments received will be reported in the Supplementary Agenda.

The objectors have been notified of the Planning Control Committee meeting.

### **Consultations**

**Traffic Section** - No objections, subject to the inclusion of conditions relating to

**Drainage Section** - Comments awaited.

**Environmental Health - Contaminated Land** - No objections, subject to the inclusion of conditions relating to contaminated land.

**Design for security** - No objections.

**United Utilities** - Comments awaited.

### **Unitary Development Plan and Policies**

EC2/1	Employment Generating Areas
EC2/2	Employment Land and Premises
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H4/1	Affordable Housing
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/5	Crime Prevention
EN1/7	Throughroutes and Gateways
EN5/1	New Development and Flood Risk
EN6	Conservation of the Natural Environment

EN6/2	Sites of Nature Conservation Interest LNR's
EN6/4	Wildlife Links and Corridors
EN7	Pollution Control
EN7/2	Noise Pollution
EN7/5	Waste Water Management
EN8	Woodland and Trees
EN8/2	Woodland and Tree Planting
RT2/2	Recreation Provision in New Housing Development
HT2/4	Car Parking and New Development
SPD1	Open Space, Sport and Recreation Provision
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

### **Issues and Analysis**

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

**Principle (Employment)** - The proposed development is located within the Eton Hill Road Employment Generating Area (EGA) and Policy EC2/1 is relevant.

Policy EC2/1 states that the Council will only allow development for Business (B1), General Industrial (B2) and Warehousing (B8) uses. Other uses will only be acceptable where they constitute limited development or would not substantially detract from the area's value for generating employment.

The proposed residential development is not one of the uses that would be acceptable within the EGA. The issue is therefore whether the proposal constitutes limited development or whether it would substantially detract from the value of the EGA.

The site is 0.46 hectares in size and would provide 18 dwellings, which is not considered to be 'limited' in the context of Policy EC2/1. The outstanding issue is therefore to consider whether the proposal would substantially detract from the area's value as an EGA.

In terms of the likelihood of the site being developed for employment uses, it is important to consider whether there has and is likely to be any interest in the site. The applicant has submitted evidence to show that the site has been marketed for a significant period and has submitted a development appraisal seeking to demonstrate that the development of the site for employment is commercially unviable. This information would appear to be reasonable.

In considering whether the proposal would substantially detract from the EGA in terms of it adversely affecting other employment uses, it is necessary to consider the relationship of the site with the rest of the EGA. In this respect, the site sits on the outer edge of the EGA and feels somewhat detached given that it is separated from the active areas of the EGA by substantial palisade fencing. In addition, the site is further detached from any active employment uses by significant areas of access and servicing areas. As such, it is considered that the proposed development would not substantially detract from the EGA and consequently, would not be in conflict with Policy EC2/1 of the Bury Unitary Development Plan and SPD14.

**Principle (Housing)** - Following revocation of the North West Regional Strategy on 20 May 2013, there is no statutory housing target for Bury. Work has commenced on the Greater Manchester Spatial Framework and this will bring forward a new statutory housing target for

the Borough. This will subsequently be incorporated into Bury's future Local Plan.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable housing land.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The site is located within the urban area and there is residential development to the west and employment land to the south. The site is previously developed land and would be located in a sustainable location with regard to public transport and services. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policy H1/2 of the Bury Unitary Development Plan and the NPPF.

**Planning obligations** - A contribution for recreation provision would be required of £50,779.08 in accordance with Policy RT2/2 of the Bury Unitary Development plan and SPD1.

The applicant has submitted a viability assessment, which indicates that the scheme would not be viable if the full contribution for recreation provision were made. The applicant has agreed to pay £37,342.00 and to enter into a 'clawback' agreement. An overage clause would be included within the Section 106 to allow the Council to meet the full policy requirements should the scheme be more viable than suggested in the viability assessment.

**Design and layout** - The proposed dwellings would be located around a central access road and would be two storeys in height. The proposed dwellings would be constructed from brick with a tile roof, which would match the surrounding properties. The use of porches, canopies and areas of render would add visual interest to the elevations. Plots 1 and 18 would take pedestrian access off Bury Road and have been designed to ensure an active frontage to Bury Road, which is welcomed. Therefore, the proposed development would not be an unduly prominent feature within the streetscene and would be in accordance with Policies H2/1, H2/2 and EN1/2 of the Bury Unitary Development Plan.

The level of private amenity space for the individual dwellings would be acceptable and there would be space within the side/rear gardens for bin storage. The proposed fencing would be either close boarded timber or a timber post and rail fence, which would be acceptable and would match existing fencing in the locality. Therefore, the proposed development would be in accordance with Policies H2/1, H2/2 and EN1/2 of the Bury Unitary Development Plan.

**Impact upon residential amenity** - SPD 6 provides guidance on aspect standards for residential properties and would be relevant in this case.

There would be over 20 metres between the plots 1 - 6 and plots 11 - 18 and there would be 27 metres between plots 7 - 8 and plot 6. This would be in excess of the 20 metre minimum required.

There would be over 27 metres between the front elevation of plots 1 and 18 and the properties on Bury Road, which would be in excess of the 20 metres required. There would be between 26 metres and 35 metres between the proposed dwellings and the industrial units to the south. This would be in excess of the 20 metres.

Therefore, the proposed development would not have an adverse impact upon the amenity of the neighbouring properties.



**Highways issues** - The proposed development would be accessed from Bury Road via a new access point. Visibility splays would be provided and the existing highway trees would be unaffected. The Traffic Section has no objections, subject to the inclusion of conditions relating to the site access, a construction management plan, turning facilities and measures to prevent mud from passing onto the highway. Therefore, the proposed development would not be detrimental to highway safety and would be in accordance with Policies H2/1 and H2/2 of the Bury Unitary Development Plan.

**Parking** - SPD11 states that the maximum parking standards is 1.5 spaces per 2 bed dwelling, 2 spaces per 3 bed dwelling and 3 spaces per 4 bed dwelling. This equates to 33 spaces.

The proposed development would provide 36 spaces, which would comply with the maximum parking standards. Therefore, the proposed development would be in accordance with Policy H2/4 of the Bury Unitary Development Plan and SPD11.

### **Response to objectors**

The issues of housing need, privacy and traffic have been dealt with in the above report. The issues of road works, dust during construction and the loss of a view are not material planning considerations and cannot be taken into consideration.

Noise - proposed dwellings over 26 metres away and landscaping to take place on the boundary. No significant adverse impact upon the amenity of the neighbouring properties.

### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

**Recommendation:** Minded to Approve

### **Conditions/ Reasons**

1. The development must be begun not later than three years beginning with the date of this permission.  
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 3345 01 B, 3345 02, 3345 05 L, 3345 08, 3345 09, 3345 10 A, 3345 11, 3345 12, R/1662/1, A089418-T001, A089418-P001 A and the development shall not be carried out except in accordance with the drawings hereby approved.  
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
  - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
  - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;

- Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.  
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. Following the provisions of Condition 3 of this planning permission, where ground gas remediation / protection measures are required, the approved Remediation Strategy must be carried out to the written satisfaction of the Local Planning Authority within approved timescales; and  
A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.  
Reason. To alleviate any possible risk associated with the production of landfill gas and ground gas in accordance with the recommendations of the Environment Agency and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
6. Details/Samples of the (materials/bricks) to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.  
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
7. No development shall commence unless or until details of a scheme for the disposal of foul and surface water has been submitted to and approved in writing by the Local Planning Authority. Only the approved scheme shall be implemented prior to occupation of the dwellings hereby approved.  
Reason. No details of the drainage have been submitted and to ensure a satisfactory means of drainage pursuant to Policy EN7/5 - Waste Water Management of the Bury Unitary Development Plan.
8. No development shall commence unless and until full details of the formation of the proposed site access onto Bury Road, incorporating the provision of appropriate tactile paving and all necessary remedial works on the adopted highway, including the relocation of all affected highway drainage, have been submitted to and agreed in writing with the Local Planning Authority. The details subsequently approved shall be implemented to an agreed programme and to the written satisfaction of the Local Planning Authority.  
Reason. To ensure good highway design and maintain the integrity of the adopted highway in the interests of highway safety pursuant to the following Policies of the

Bury Unitary Development Plan:  
Policy H2/1 - The Form of New Residential Development  
Policy H2/2 - The Layout of New Residential Development  
Policy EN1/2 - Townscape and Built Design.

9. No development shall commence unless and until a Construction Traffic Management Plan (CTMP) has been submitted to and agreed in writing with the Local Planning Authority. The approved plan shall be adhered to throughout the construction period and shall ensure that there is provision to be made for the turning and manoeuvring of vehicles within the curtilage of the site, parking on-site of operatives and construction vehicles together with storage on-site of construction materials. The provision shall be retained and used for the intended purpose for the duration of the construction period and the areas identified shall not be used for any other purposes other than the turning and parking of vehicles and storage of construction materials respectively.  
Reason. To ensure that adequate turning facilities, car parking provision and materials storage arrangements are provided for the duration of the construction period, in the interest of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:  
Policy H2/1 - The Form of New Residential Development  
Policy H2/2 - The Layout of New Residential Development  
Policy EN1/2 - Townscape and Built Design.
10. The turning facilities indicated on approved plan reference 3345 05 Revision L shall be provided before the development is first occupied and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times.  
Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to the following Policies of the Bury Unitary Development Plan:  
Policy H2/1 - The Form of New Residential Development  
Policy H2/2 - The Layout of New Residential Development  
Policy EN1/2 - Townscape and Built Design.
11. Minimum hardstandings lengths of 5m measured from the back of the proposed private footways shall be provided to the written satisfaction of the Local Planning Authority and thereafter maintained.  
Reason. To enable a vehicle to stand clear of the highway whilst the garage doors are opened and to allow adequate space to maintain a vehicle clear of the highway in the interests of road safety pursuant to Policy H2/3 - Extensions and Alterations of the Bury Unitary Development Plan and associated Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties.
12. Before the development is commenced, details shall be submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations. The approved details shall be implemented and maintained thereafter during the period of construction.  
Reason. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Policy H2/2 - The Layout of New Residential Development of the Bury Unitary Development Plan.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**





58324

Photo 1



Photo 2



Photo 3







PROPOSED LEVELS AND BOUNDARIES SHOWN ARE INDICATIVE AND ARE TO BE CONFIRMED BY THE ENGINEER

SCHEDULE OF ACCOMMODATION

TYPE	SIZE m <sup>2</sup>	QUANTITY
A1 3 BED/5 PERSON HOUSE	78.90	4
A2 3 BED/5 PERSON HOUSE	75.00	2
B 2 BED/4 PERSON HOUSE	61.52	10
C 4 BED/7 PERSON HOUSE	99.00	2
<b>TOTAL</b>		<b>18</b>

DRAWING TO BE READ IN CONJUNCTION WITH LANDSCAPE ARCHITECT'S DRAWING R/1662/1 AND HIGHWAYS CONSULTANT'S DRAWING A089418-P001

work to figured dimensions  
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Southdale Construction  
 Residential Development  
 Land at Eton Business Park  
 Eton Hill Road  
 Radcliffe

Proposed Site Plan  
 Scheme SK04

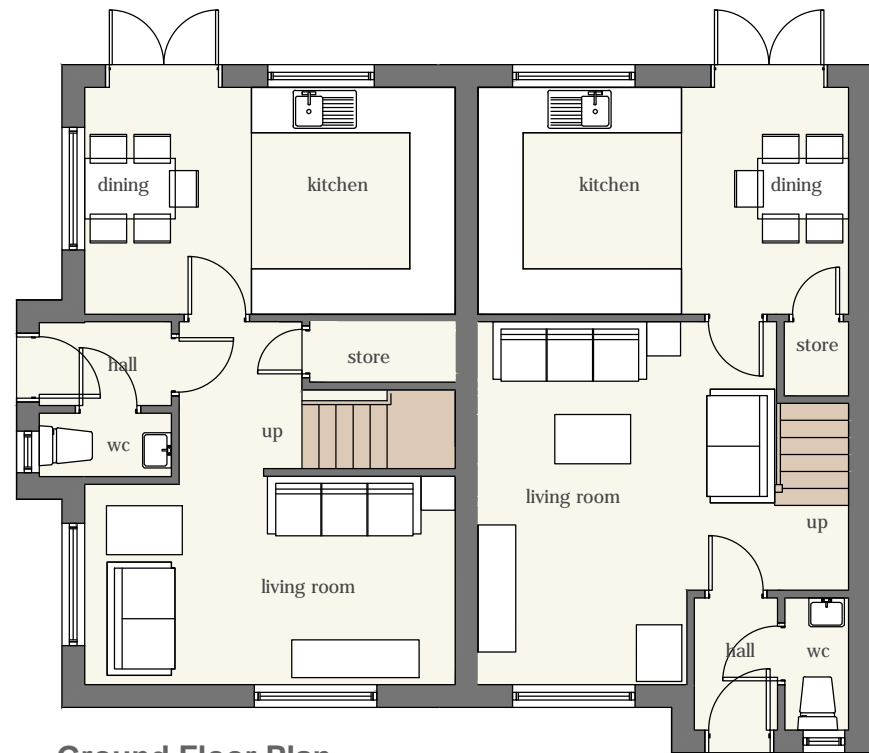


contract no  
**3345 05**  
 drawing no  
**05**  
 revision  
**L**  
 drawn by  
 PDC  
 date  
 Nov 13  
 scale @ A1  
 1:200

Revisions:  
 A. All B-type units amended to be semi-detached, and units to entrance arranged to face Bury Road. PDC March 2014  
 B. Layout amended to provide some garden frontages. PDC April 2014  
 C. Layout amended to BMC comments. PDC May 2014  
 D. Layout amended to assumed road layout of existing industrial units. June 2014  
 E. Outline of canopy and root extents added. PDC September 2014  
 F. Units adjacent to Bury Road moved to avoid root extents/canopy of existing tree. PDC September 2014  
 G. Road Layout amended to accommodate extended turning head. PDC October 2014  
 H. Highway width increased to 5.50m, and turning head amended. PDC November 2014  
 J. Turning head amended. PDC November 2014  
 K. Finished floor level added to each block. DIC 15.01.15  
 L. Amendments made to satisfaction of local authority planning department. Alterations in accordance with highways consultant's drawing. DIC 20.05.15

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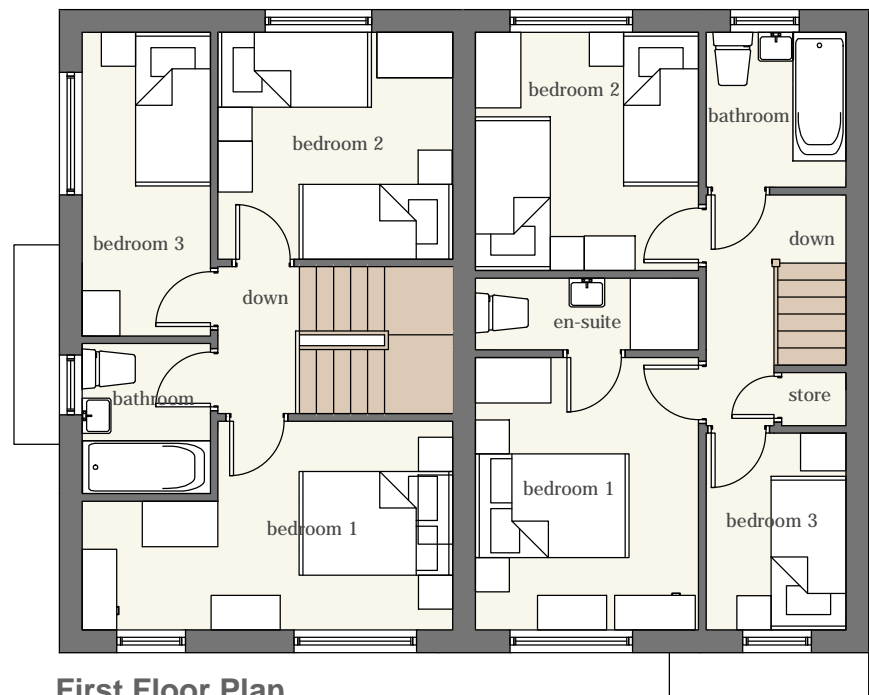
Ground Floor Plan



Bury Road Elevation



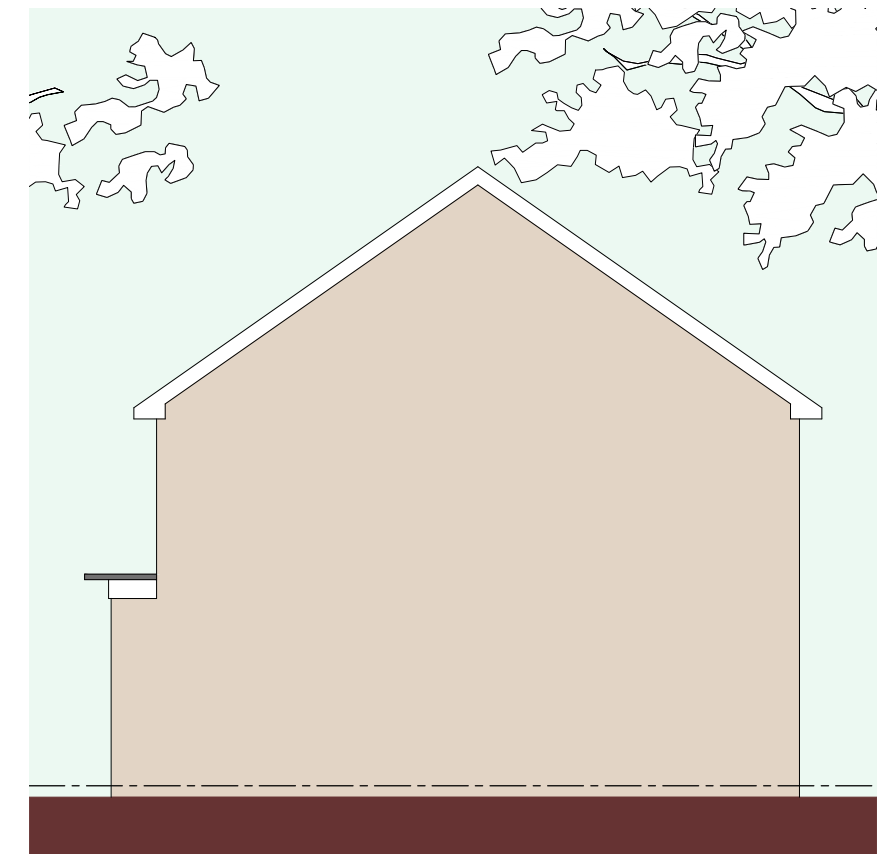
Front Elevation



First Floor Plan



Rear Elevation



Side Elevation

Southdale Construction  
Residential Development  
Land at Eton Business Park  
Eton Hill Road  
Radcliffe

Proposed Plans SK04  
House Type A1 (78.9sqm)

work to figured dimensions

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contract no

3345

drawn by

Ordnance Survey

drawing no

08

date

August 2014

revision

scale @ A3

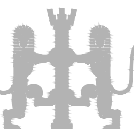
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Revisions:

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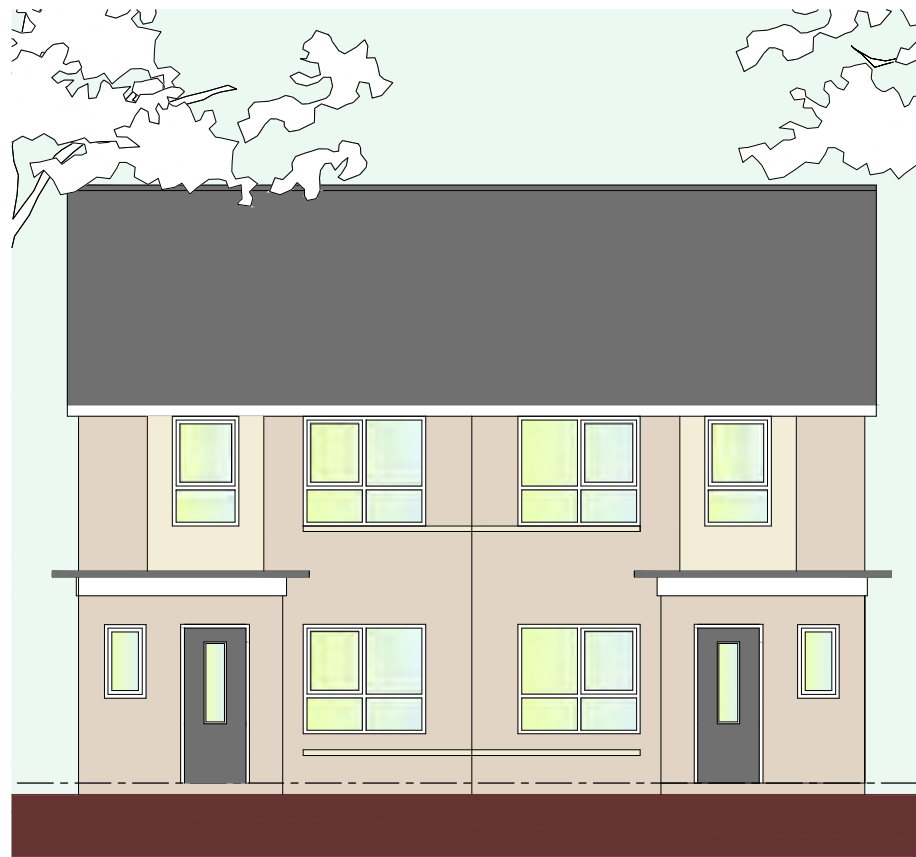
CJ PARTNERSHIP  
ENVIRONMENTAL ARCHITECTS



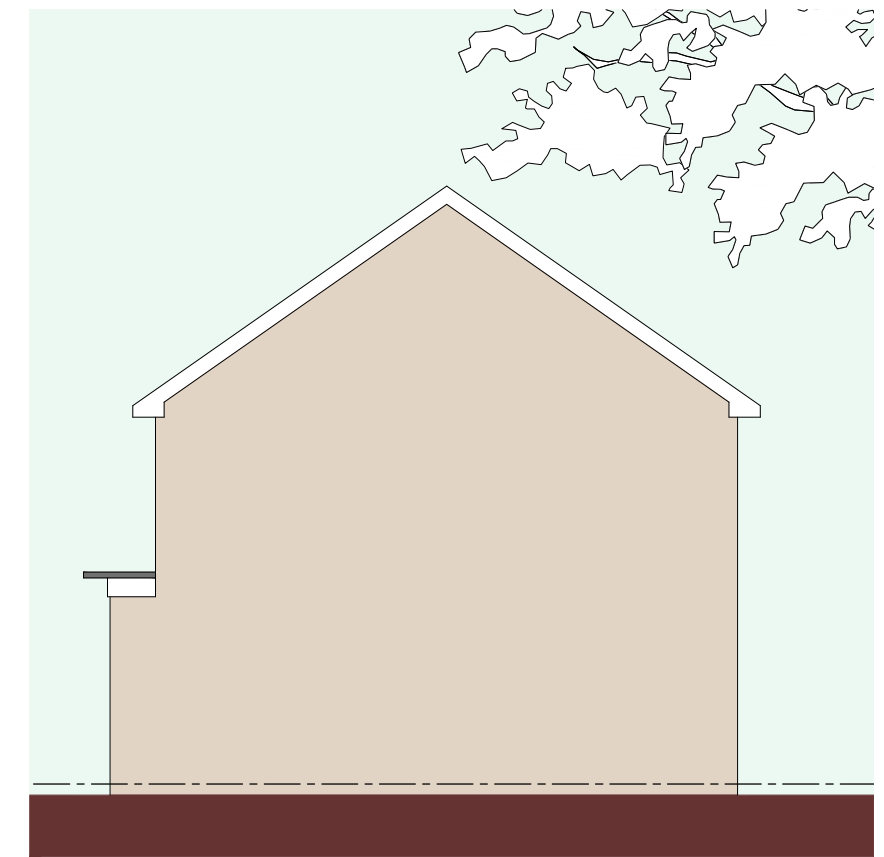




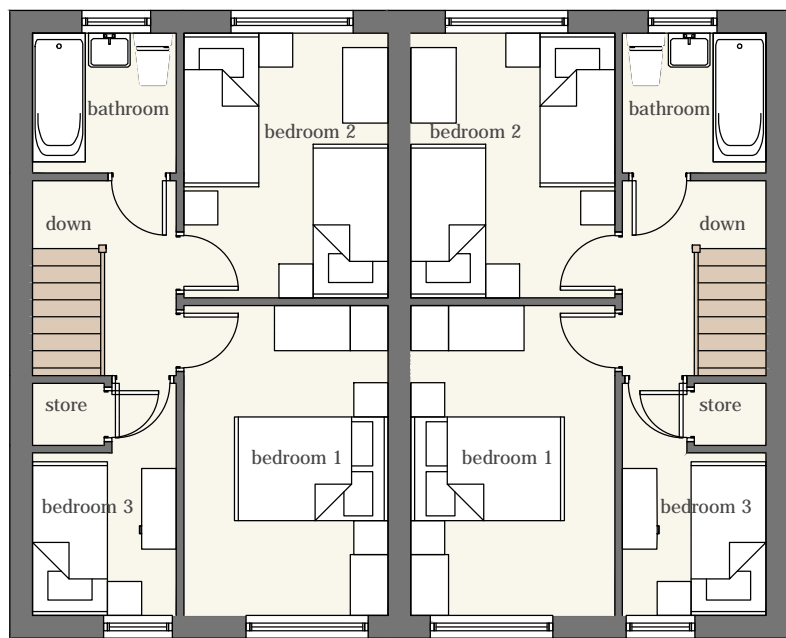
Ground Floor Plan



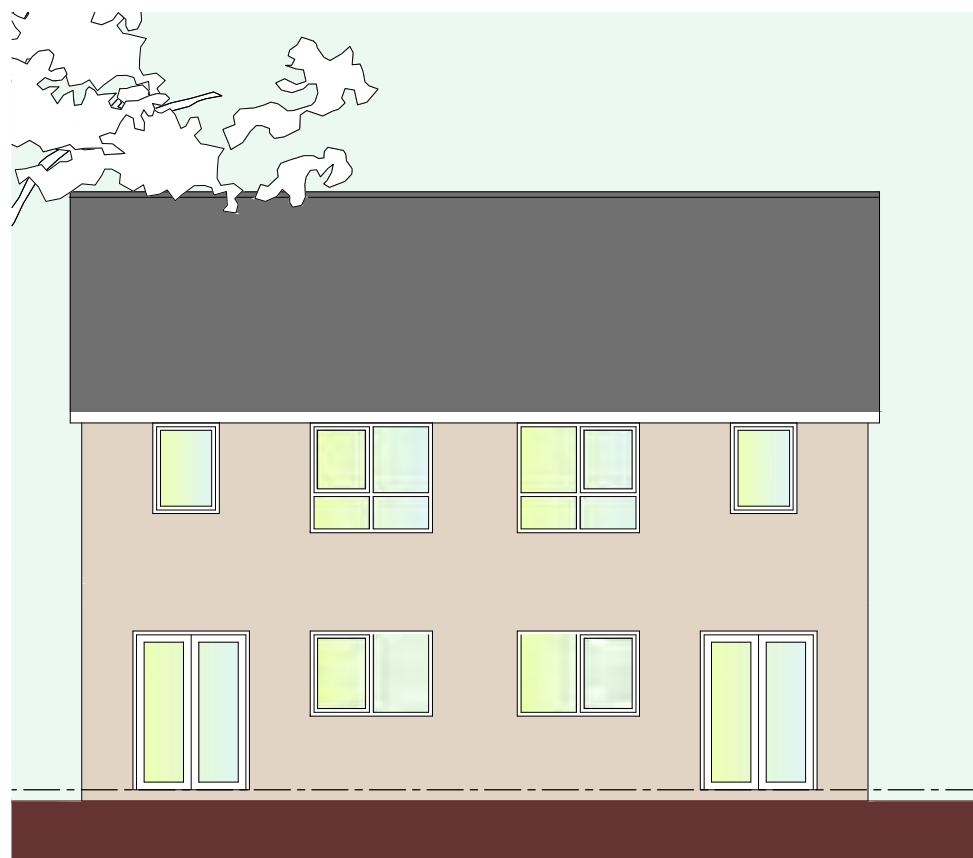
Front Elevation



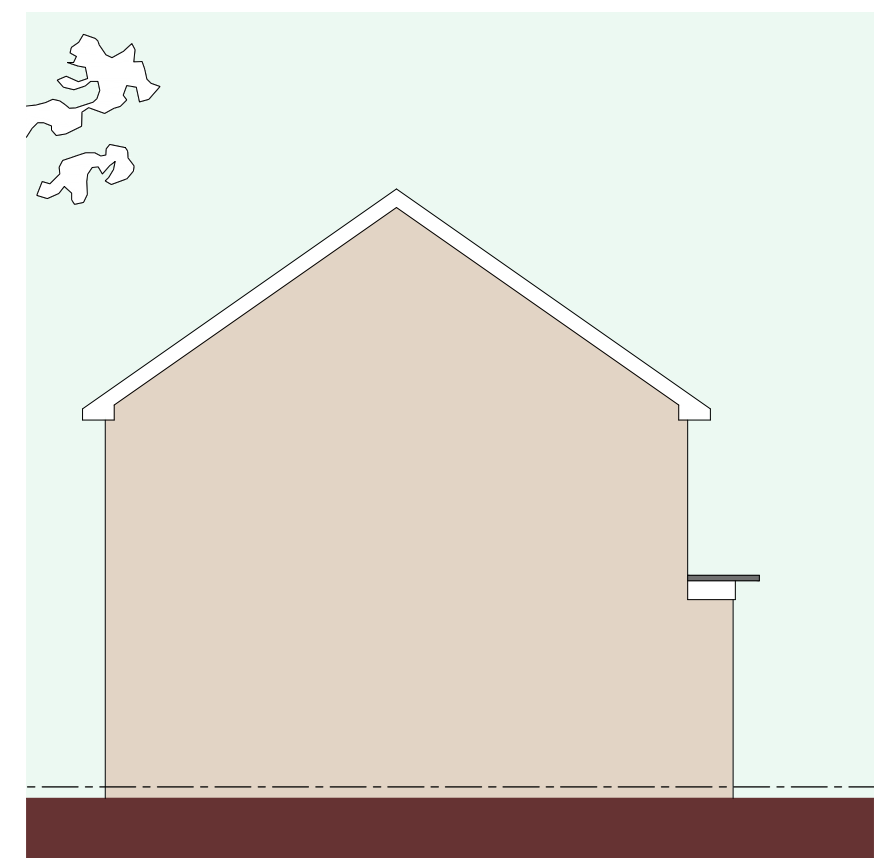
Side Elevation



First Floor Plan



Rear Elevation



Side Elevation

Southdale Construction  
Residential Development  
Land at Eton Business Park  
Eton Hill Road  
Radcliffe

Proposed Plans SK04  
House Type A2 (75sqm)

work to figured dimensions

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contract no

3345

drawn by

Ordnance Survey

drawing no

09

date

August 2014

revision

scale @ A3

1:100

Revisions:

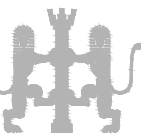
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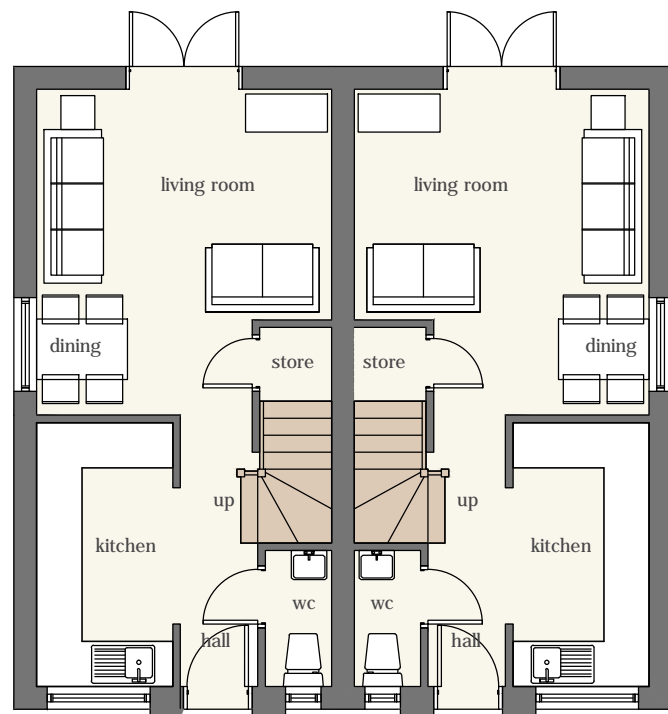
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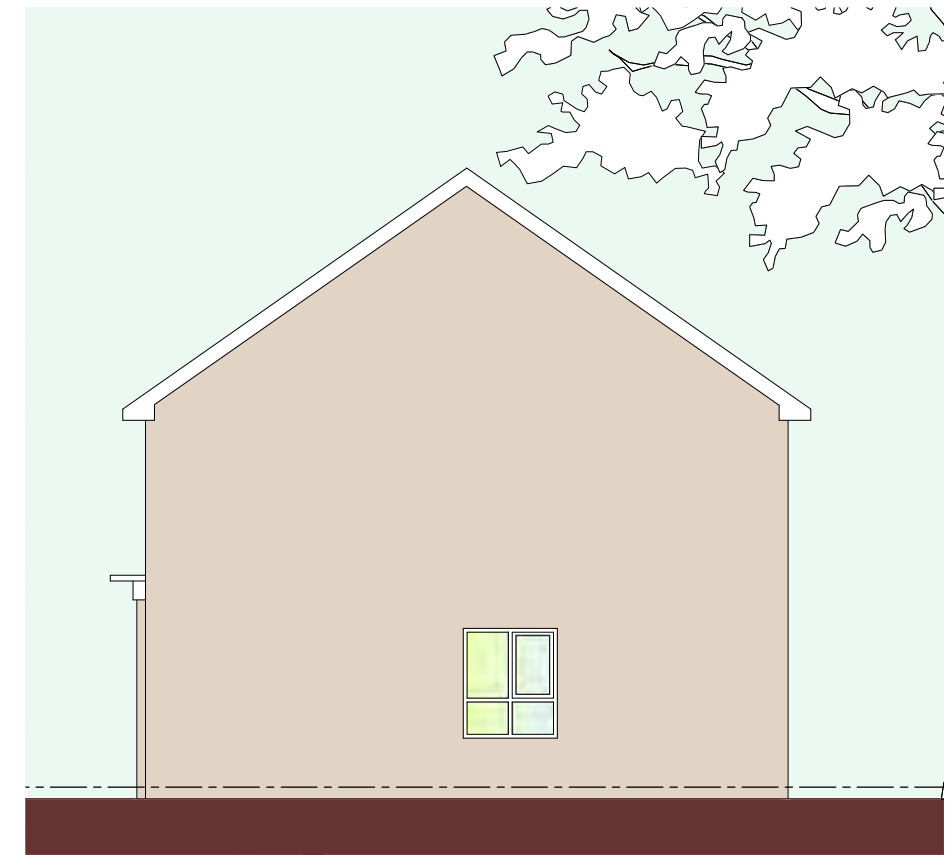




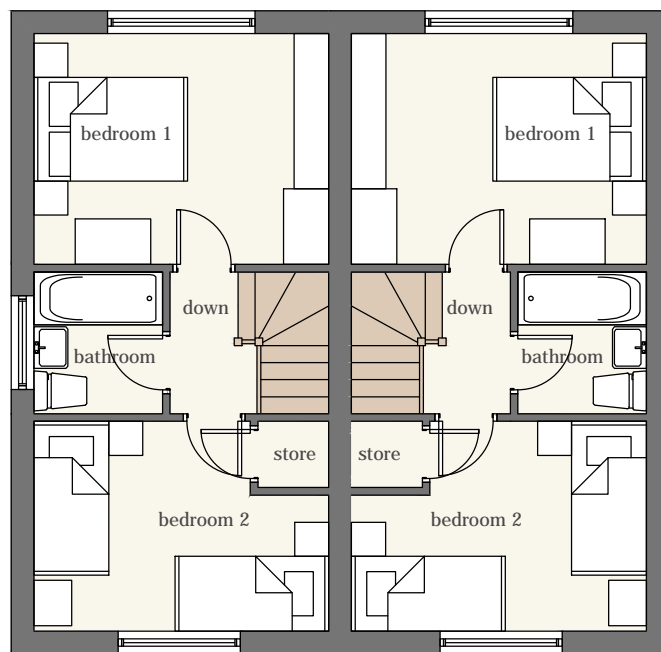
Ground Floor Plan



Front Elevation



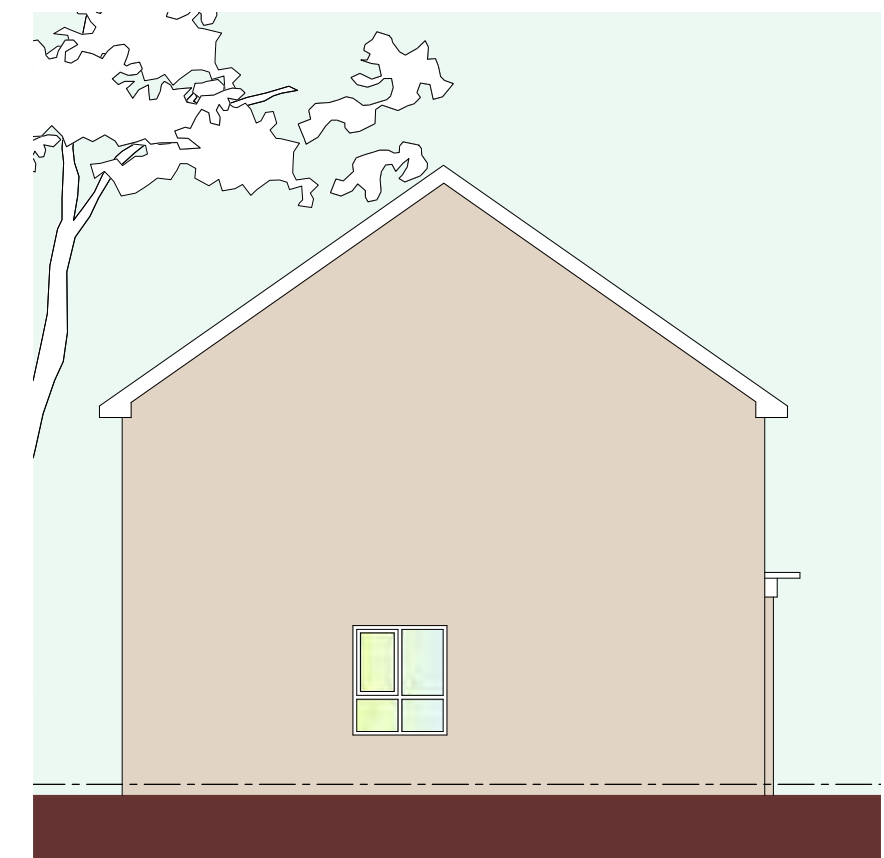
Side Elevation



First Floor Plan



Rear Elevation



Side Elevation

Southdale Construction  
Residential Development  
Land at Eton Business Park  
Eton Hill Road  
Radcliffe

Proposed Plans SK04  
House Type B (61.52sqm)

work to figured dimensions

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revision

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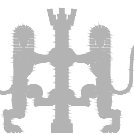
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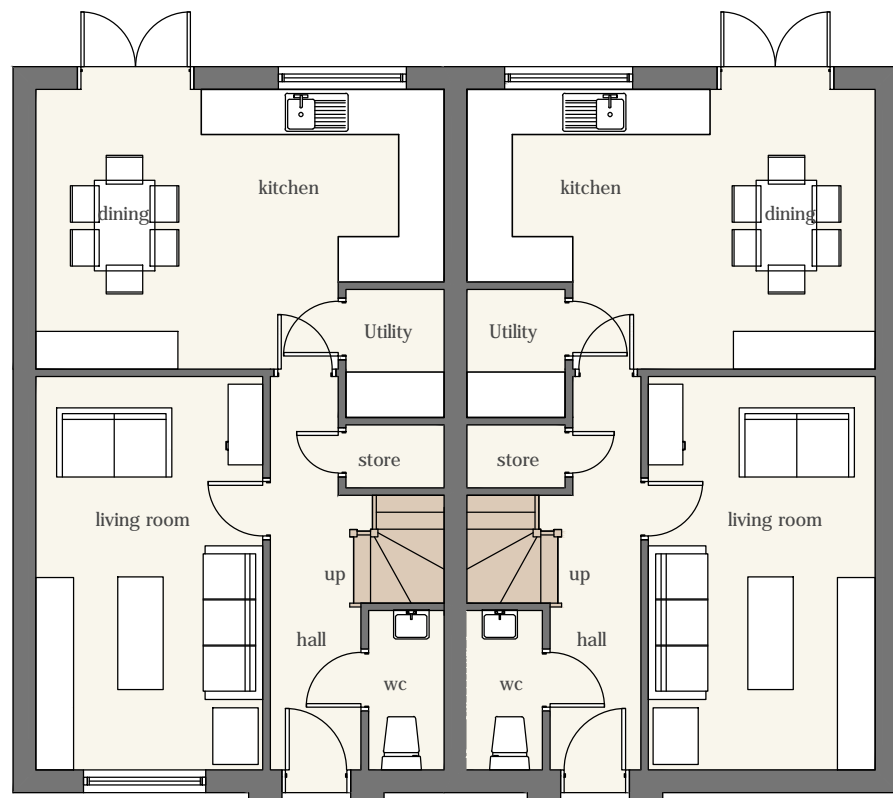
Revisions:  
A. Kitchen wall and soor removed as requested by GP.  
PDC 17.09.2014

Blue Pit Mill Queensway  
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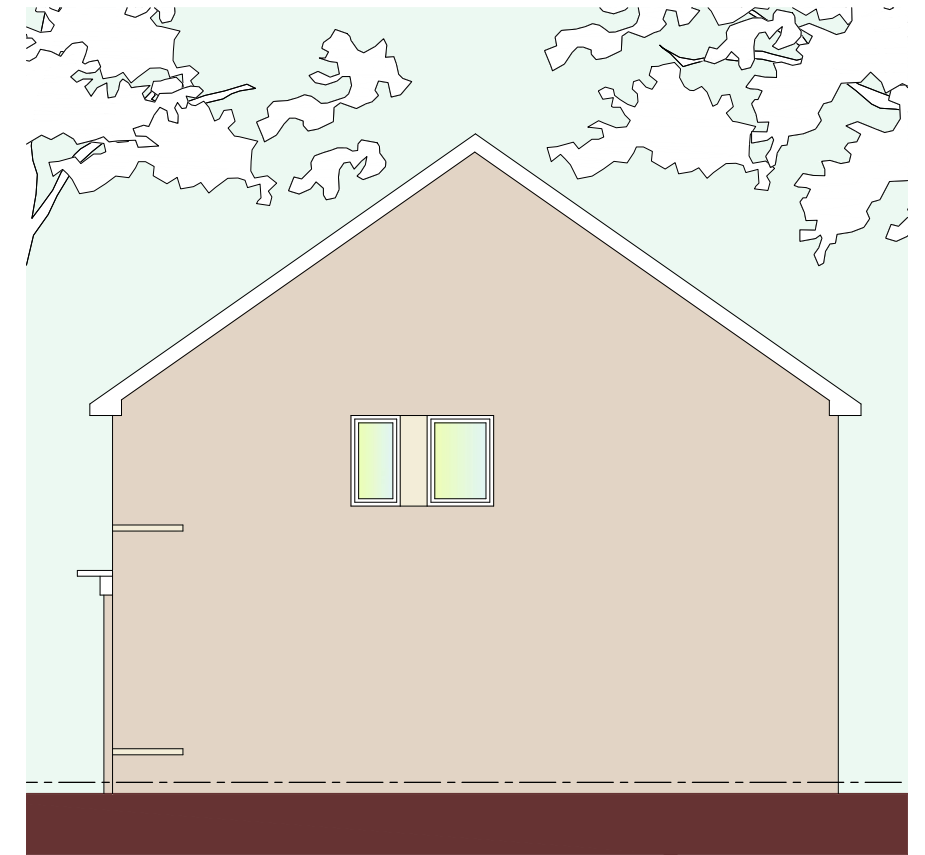




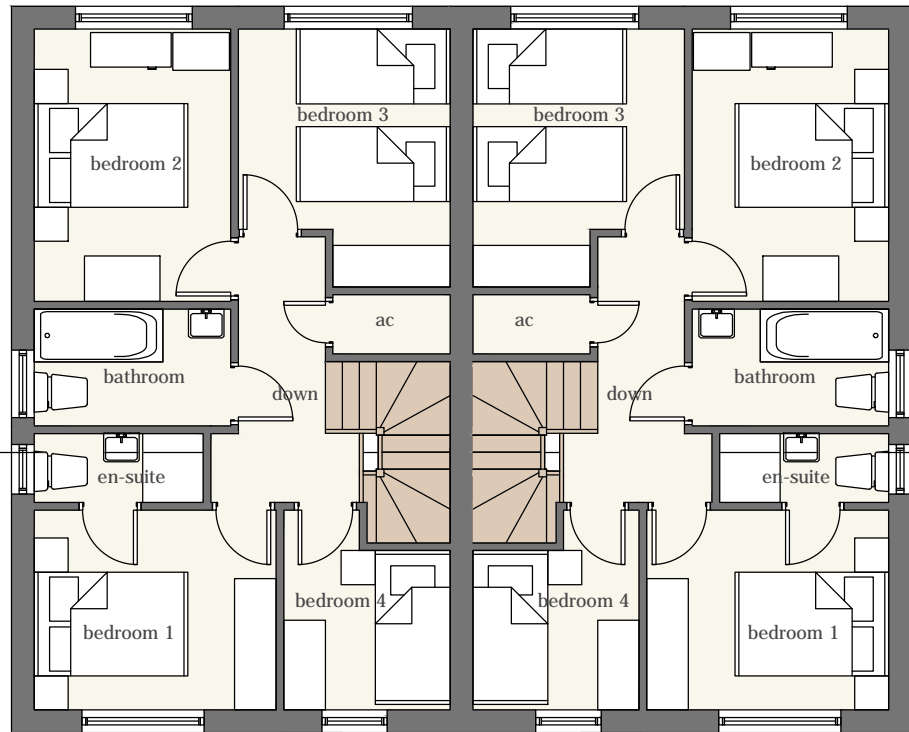
Ground Floor Plan



Front Elevation



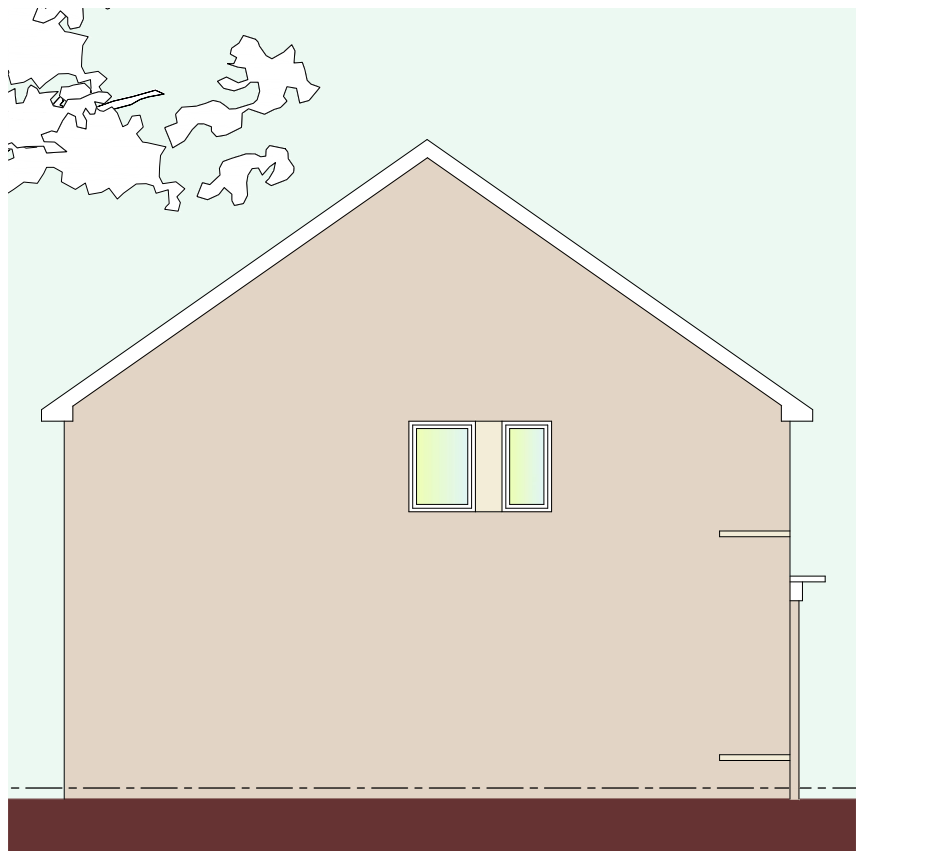
Side Elevation



First Floor Plan



Rear Elevation



Side Elevation

Southdale Construction  
Residential Development  
Land at Eton Business Park  
Eton Hill Road  
Radcliffe

Proposed Plans SK04  
House Type C (99sqm)

work to figured dimensions

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Revisions:

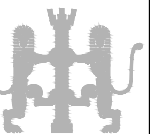
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ENVIRONMENTAL ARCHITECTS



**NOTES**

**PROTECTION OF EXISTING VEGETATION**

Existing vegetation to be retained on site shall be protected where necessary during works by a tree protective barrier, secured to a scaffold tubing framework with fluorescent tape to the top rail. Protective fencing shall be erected on the canopy drip line whenever possible. Laminated, waterproof A4 signs shall be fixed to the fence posts at 1.0m intervals bearing the words 'PROTECTED TREE ZONE - NO STORAGE OR OPERATIONS WITHIN FENCED AREA'. Tracking of machinery, storage of chemicals and building materials shall not be permitted within the protected area. Leaks or spills should be removed immediately and the contaminated soil replaced. No bonfire shall be lit within 5m of the canopy spread. Any excavation work beneath the canopy spread shall be carried out by hand.

All works affecting trees within the development shall be subject to BS 5837:2012.

Where existing vegetation lies within rear garden the area beneath the trees will be cleared of invasive scrub, lightly cultivated and sown with a shade tolerant grass seed mix.

**TREE PLANTING**

Extra heavy standard trees will be planted along Eton Hill Road to supplement the existing street trees. Heavy standard trees will be planted within frontages to soften and break up the hard landscape and provide focal points within the scheme.

Foundation design of new buildings shall accommodate proposed tree planting in accordance with NERC guidelines.

Species to be selected from:

- Acer campestre (AC)
- Alnus cordata (AC)
- Betula 'Jacquemontii' (BJ)
- Malus sibirica (MK)

- Prunus sibirica 'Autumnalis' (PSA)
- Sorbus aria 'Majestica' (SAH)
- Sorbus 'Joseph Rock' (SR)

**SHRUB PLANTING**

A mix of evergreen and deciduous shrubs will be planted in selected front gardens to soften the development and to provide a valuable food source for birds and invertebrates.

Species to be selected from: ( ) density/m<sup>2</sup>

- Berberis filicaris 'Aristoteles' (B), Berberis thunbergii 'Atropurpurea Nana' (S), Berberis cordifolia 'Silberlicht' (6), Buddleja davidii 'Lochinchy' (2), Carex obovata repens (4), Chaenactis superba 'Joc Trill' (4), Cornus alba 'Spectra' (2), Cornus stolonifera 'Flairframe' (2), Cotoneaster conspicuus 'Decorat' (6), Cotoneaster lacteus (2), Cydonia 'Alpokr' (4), Elaeagnus pungens 'Maculata' (3), Escallonia 'Aylee Bloom' (4), Euonymus fortunei 'Emerald Gaiety' (6), Ficus sp. (6), Geacranium 'Johnson's Blue' (4), Hebe allicans 'Red Edge' (4), Hebe 'Autumn Glory' (4), Hypericum x westlandicum (4), Jaspisana salina 'Tamarisfolia' (3), Lavandula 'Hidcot' (6), Loropetalum 'Meygreen' (4), Loropetalum 'Beauty' (4), Nepeta faassenii (4), Perovskia 'Blue Spire' (6), Phormium var. 'Fountain' (4), Prunus laurocerasus 'Zabelliana' (3), Pyracantha 'Orange Charm' (3), Rubus x 'Betty Ashburner' (4), Salix 'Gold Flame' (4), Spiraea 'Shirobana' (4), Viburnum lonicera 'Eve Price' (3)

**HEDGE PLANTING**

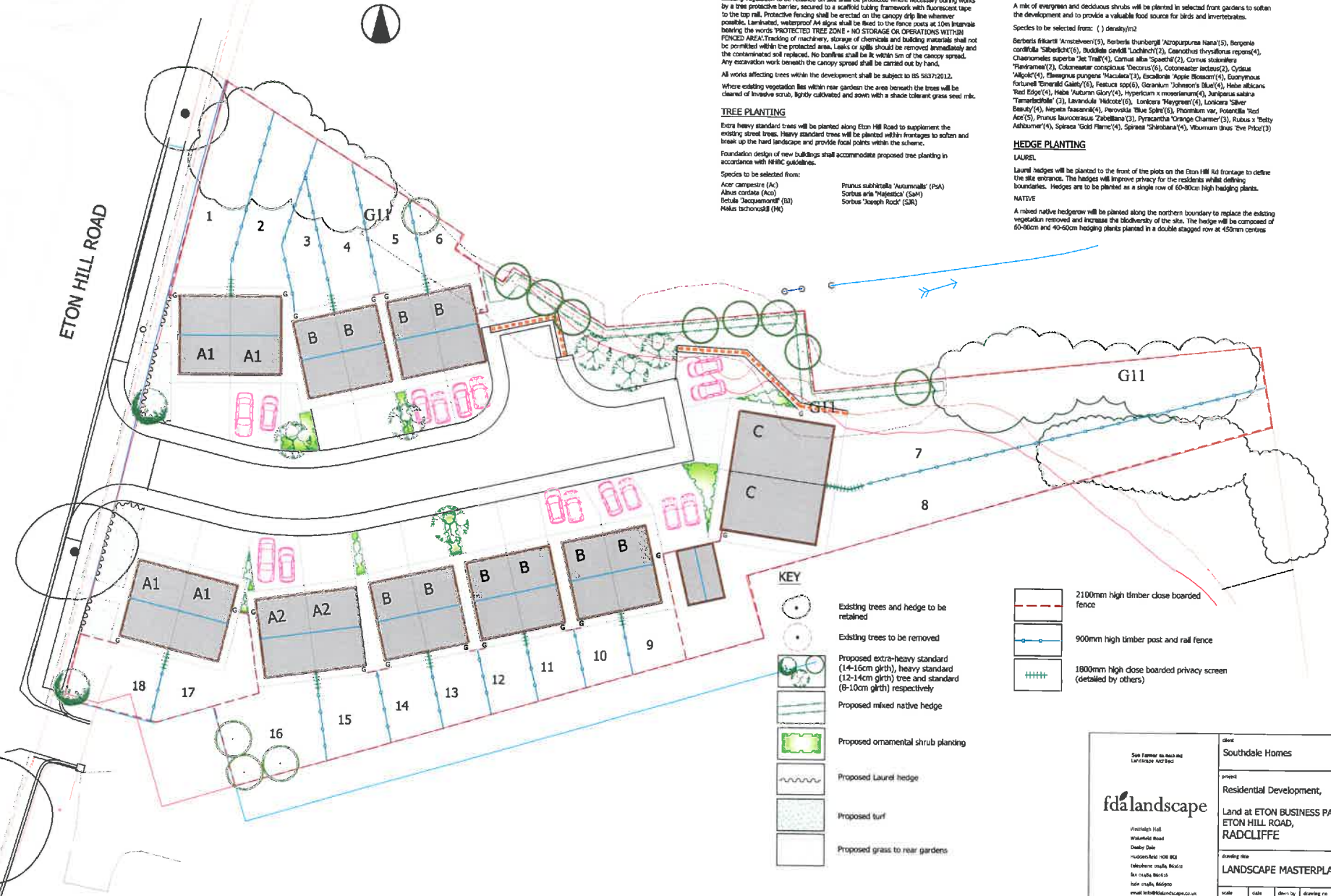
**LAUREL**

Laurel hedges will be planted to the front of the plots on the Eton Hill Rd frontage to define the site entrances. The hedges will improve privacy for the residents whilst defining boundaries. Hedges are to be planted as a single row of 60-80cm high planting plants.

**NATIVE**

A mixed native hedgerow will be planted along the northern boundary to replace the existing vegetation removed and increase the biodiversity of the site. The hedge will be composed of 60-80cm and 40-60cm hedging plants planted in a double staggered row at 450mm centres.

ETON HILL ROAD



**KEY**

- Existing trees and hedge to be retained
- Existing trees to be removed
- Proposed extra-heavy standard (14-16cm girth), heavy standard (12-14cm girth) tree and standard (8-10cm girth) respectively
- Proposed mixed native hedge
- Proposed ornamental shrub planting
- Proposed Laurel hedge
- Proposed turf
- Proposed grass to rear gardens

- 2100mm high timber close boarded fence
- 900mm high timber post and rail fence
- 1800mm high close boarded privacy screen (detailed by others)

See Form 9a and 9b LANDSCAPE ARCHITECT		client Southdale Homes
project Residential Development, Land at ETON BUSINESS PARK ETON HILL ROAD, RADCLIFFE		drawing title LANDSCAPE MASTERPLAN
1100000 Hill 1100000 Hill Derby Dale 1100000 Hill 1000 1000 telephone 01454 860000 fax 01454 860000 info@fdalandscape.com email info@fdalandscape.com www.fdalandscape.com	scale 1:200 OAL	date Nov 14 SF
drawn by R/1662/1	checked by R/1662/1	drawing no R/1662/1

**Ward:** Prestwich - St Mary's

Item 02

**Applicant:** Redrow Homes (Lancashire) Ltd

**Location:** Land at Valley Park Road/Clifton Road Prestwich, Manchester, M25 3TG

**Proposal:** Erection of 97 no. residential units (Class C3) comprising 67 no. dwellings and 30 no. apartments with associated landscaping, access arrangements and car parking and substation and creation of pond and woodland walk and seating area

**Application Ref:** 58655/Full

**Target Date:** 05/08/2015

**Recommendation:** Minded to Approve

**It is recommended that this application is Minded to Approve subject to the signing and completion of a Section 106 agreement for recreation provision in accordance with Policy RT2/2 of the adopted Unitary Development Plan and SPD1 and for Affordable Housing in accordance with Policy H4/1 of the UDP and SPD5. If the agreement is not signed within a reasonable timeframe, then delegated authority is sought by the Development Manager to determine the application.**

**A site visit was undertaken on 1 September 2015. This item was deferred to allow further negotiations to take place to provide a better mix of affordable housing throughout the site.**

### **Description**

The site is a triangular piece of land, which is located to the west of the Tesco store. The site originally formed part of Prestwich Hospital and formed part of the development for the retail store, restaurant and hotel, which was allowed on appeal in 1993. The site is vacant with a number of trees within the site. Access is taken from Valley Park Road and Pinfold Drive. There is an access (Thornton Crescent) which crosses the site from west to east and connects to Clifton Road.

Prestwich Hospital is located to the north and west of the site and the Tesco store is located to the east. There is an existing football pitch to the south with Clifton Road and residential properties beyond. There are residential properties to the southeast (on Kingswood Road/West Road).

The proposed development involves the erection of 97 residential units, which comprises 67 dwellings and 30 apartments. All the proposed dwellings would be two storeys in height and there would be a mix of detached, semi-detached and terraced properties. The proposed apartments would be located in two buildings, which would be three storeys in height. The proposed dwellings and apartments would be constructed from red brick, render with timber detailing and a tile roof. Access to the site would be taken from Valley Park Road and Pinfold Drive.

### **Relevant Planning History**

25004 - Proposed shopping, housing and hotel development at Prestwich Hospital, Bury New Road, Prestwich. Refused - 20 December 1990. Allowed on appeal - 5 April 1993.

29082 - Erection of retail food store, petrol filling station including car parking, landscaping and alterations to Bury New Road - Approval of reserved matters at Prestwich Hospital, Bury New Road, Prestwich. Approved with conditions - 23 December 1993.

31839 - Renewal of outline planning permission for residential element of retail store, petrol

filling station, residential and hotel development at Former Prestwich Hospital, Bury New Road, Prestwich. Approved with conditions - 28 March 1996.

43686 - Clubhouse with changing facilities and car parking; new lower tier with junior pitches and countryside walk at land at Prestwich hospital, Bury New Road, Prestwich. Approved with conditions - 8 July 2005.

### **Publicity**

318 neighbouring properties were notified by means of a letter on 6 May 2015 and a press notice was published in the Bury Times on 14 May. Site notices were posted on 12 May 2015.

43 letters have been received from the occupiers of 6 North Road, 1, 2, 5, 6, 9, 16 West Road, Cheddleton Lodge, 3, 5, 20, 24, 26 Dashwood Road, 49, 56, 70, 71, 74, 78, 89, 99 Clifton Road, 1, 27, 29, 37 Kingswood Road, 42 Haslam Hey Close and 11 Wyndham Park, which have raised the following issues:

- Concerned about the proposed walkway connecting Kingswood Road to the development. This should connect to the recently approved residential development on Kingswood Road.
- Concerned about the loss of trees.
- Object to the three storey apartment blocks, which will overlook West Road and Cheddleton Lodge.
- Object to the proposed substation being located near an existing dwelling and the associated health risks they bring.
- The proposal will lead to increased traffic in the area and would be a danger for children playing in the road.
- Increased noise pollution.
- Increased light pollution from the three storey flats.
- The proposed emergency access would destroy a magnificent Victorian boundary wall and would cause trees to be destroyed.
- The current strain on school places and GPs is a major concern.
- The proposal is in contravention of the Bury UDP as it is to the detriment of the quality, character and amenity value of the area.
- The proposed emergency access should connect to the rear of the existing car park at Tesco or the existing road at Clifton Road.
- Concerned that the emergency access road would be used regularly by owners of the new homes.
- The proposed block of apartments would be 11.5 metres in height and would be much taller than the existing two storey dwellings.
- The applicant has not considered the amenity of the existing properties, only those within the proposed development.
- The proposed development would contravene our rights under Protocol 1, Article 1 of the Human Rights Act, as it would curtail the right to peaceful enjoyment of our home and surroundings.
- Any construction traffic must use Valley park Road and not West Road, Kingswood Road or Clifton Road.
- Impact upon ground stability and drainage in Prestwich.
- The leafy character of the area would be lost.
- The proposed development does not reflect the local context, the street pattern or the scale and proportions of local buildings.
- The proposed apartment block would cause significant noise, which would disturb the existing residents.
- The additional permeability provided by the proposed emergency access at Clifton Road is not necessary and would not be a viable option for an emergency due to increased congestion.
- The Tree Survey does not guarantee that the opening of the access point will not damage T28 (lime tree).
- The proposed emergency access would become the primary pedestrian access point



and would cause an increase in noise and would have a significant adverse impact upon the residents of the surrounding area.

- There is a lack of information as to how the proposed emergency access will be controlled.
- The emergency access may be used as a full access to the site at a later date. A small spur has been added to the main road adjacent to the emergency access to accommodate this use.
- HGVs already use Kingswood Road due to satnavs and poor map reading.
- The application has not received any scrutiny from the Council prior to consultation. I reasonably expect to see the Council's preliminary observations in the consultation letter to confirm that the proposal is worthy of my consideration.
- In its present form this application is UNACCEPTABLE. *Redrow* must be REQUIRED to reconsider its submission with considerable rigour and INSTRUCTED to do so by a Council which takes seriously its primary function to serve its communities.
- Object to the three storey apartments, but would have no objections to two storey apartments.
- There will be an additional risk of flooding from surface water run off.
- This area of Prestwich is known as the 'Village' because of the character of the area and it seems that this is being destroyed by the Council which is seeking to continually keep chipping away at the area to the detriment of local residents.
- The proposed apartments would be 2 metres above the existing levels and as such, would be higher than the proposed elevation plan suggests.
- The topographical survey did not include the properties on West Road and as such, it is not possible to ascertain whether there are adequate aspect distances in place.
- The mature trees would offer little screening during the winter months.
- The photographs within the design and access statement are incorrectly labelled and the immediate properties are two storeys in height.
- The proposed development does not comply with SPD16 and should be refused.

The neighbouring properties were notified of revised plans by means of a letter on 16 September 2015.

Any comments received will be reported in the Supplementary Report.

The objectors have been notified of the Planning Control Committee meeting.

### **Consultations**

**Traffic Section** - No objections, subject to the inclusion of conditions relating to highway layout, traffic regulation measures, construction management plan, visibility splays, turning facilities, parking for operatives and measures to prevent mud from passing onto the highway.

**Drainage Section** - No objections, subject to the inclusion of a condition relating to flood risk and surface water drainage.

**Environmental Health - Contaminated Land** - No objections, subject to the inclusion of conditions relating to contaminated land.

**Environmental Health - Pollution Control** - Comments awaited and will be reported in the Supplementary Report.

**Waste Management** - No objections.

**Public Rights of Way Officer** - Comments awaited and will be reported in the Supplementary Report.

**Children's Centres & Early Years** - Comments awaited and will be reported in the Supplementary Report.

**Performance & Housing Strategy** - Comments awaited and will be reported in the Supplementary Report.

**Prestwich Township Forum** - Comments awaited and will be reported in the Supplementary Report.

**GM Ecology Unit** - No objections, subject to the inclusion of conditions relating to tree protection methods, Japanese Knotweed and Himalayan Balsam, and a habitat management plan.

**Environment Agency** - No comments.

**Electricity North West** - No objections, subject to the inclusion of informatives relating to infrastructure.

**United Utilities** - No objections, subject to the inclusion of a condition relating to foul and surface water drainage.

**The Coal Authority** - No objections, subject to the inclusion of an informative relating to coal mining.

**GM Fire Service** - No objections, subject to the inclusion of conditions.

**Transport for Greater Manchester** - No objections, subject to the inclusion of a condition relating to a travel plan.

**Minerals and Waste Planning Unit** - No objections.

**Highways Agency** - No objections, subject to the inclusion of a condition relating to a travel plan.

**Designforsecurity** - Comments awaited and will be reported in the Supplementary Report.

### **Unitary Development Plan and Policies**

H1/1	Housing Land Allocations
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H4/1	Affordable Housing
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/5	Crime Prevention
EN5/1	New Development and Flood Risk
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN6/4	Wildlife Links and Corridors
EN7	Pollution Control
EN7/5	Waste Water Management
EN8	Woodland and Trees
EN8/1	Tree Preservation Orders
EN8/2	Woodland and Tree Planting
RT1/1	Protection of Recreation Provision in the Urban Area
RT2/2	Recreation Provision in New Housing Development
HT2/4	Car Parking and New Development
HT4	New Development
HT5/1	Access For Those with Special Needs
HT6/2	Pedestrian/Vehicular Conflict
SPD1	Open Space, Sport and Recreation Provision
SPD5	DC Policy Guidance Note 5: Affordable Housing
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
SPD16	Design and Layout of New Development in Bury
NPPF	National Planning Policy Framework
NPPG	National Planning Policy Guide

### **Issues and Analysis**

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

**Principle** - Following revocation of the North West Regional Strategy on 20th May 2013,



there is no statutory housing target for Bury. Work is in progress on Bury's Local Plan - 'The Core Strategy', which will bring forward a new statutory housing target. It is currently proposed that the new housing target in the Local Plan should be set at 400 dwellings per annum, which is currently timetabled to be adopted in 2014.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable housing land.

The proposed dwellings would be located on land, which is allocated for residential development under Policy H1/1.

The site is located within the urban area and is adjacent to a residential area. The proposed development would not conflict with the surrounding uses and is in a sustainable location with good access to public transport and services. The site formed part of the adjacent hospital and contained buildings and is previously developed land. Therefore, the proposed development would be in accordance with Policy H1/1 of the Bury Unitary Development Plan and the NPPF.

**Design and layout** - The proposed development would include a mix of detached, semi-detached, terraced dwellings and flats. The proposed dwellings would be constructed from a mix of materials, including red brick, render with timber detailing and a tile roof. The proposed materials would match the surrounding area. The use of headers, cills, brick banding, canopies and pike details would add visual interest to the elevations. As such, the proposed dwellings would not be a prominent feature within the locality.

All of the proposed dwellings would have a side or rear garden, which would provide a suitable level of private amenity space. There would be space within the rear or side gardens for bin storage. The proposed boundary treatments would include a 1.8 metre high timber boarded fence, which would be acceptable and would match the existing fencing in the area.

Therefore, the proposed development would be in accordance with Policies EN1/2, H2/1 and H2/2 of the Bury Unitary Development Plan.

**Impact upon residential amenity** - SPD6 provides guidance on aspect standards between residential properties and as such, would be relevant in this case. The aspect standards states that there should be a minimum of 20 metres between directly facing habitable windows and 13 metres between a habitable room window and a two storey blank wall. For each additional storey in height, 3 metres should be added to the separation distance, i.e. there should be 23 metres between directly facing habitable room windows.

There would be 33 metres between the proposed apartment building and the properties on West Road, which would be in excess of the 23 metre distance required. There would be at least 28 metres between the proposed apartments and Cheddleton House, which would be in excess of the required aspect standard.

Plots 80 - 86 and 88 - 95 would back onto land owned by Prestwich Hospital. All of the plots would have a rear garden of at least 10 metres, which would result in the aspect standard being shared between the two sites. The remainder of the proposed development would comply with or be in excess of the relevant aspect standards.

Therefore, the proposed development would not have an adverse impact upon the residential amenity of the neighbouring properties.

**Trees** - All of the trees within the site are protected by an area Tree Preservation Order and a tree survey has been submitted as part of the application. The trees have not been

actively managed for a number of years and as such, many are of poor quality.

It is proposed to remove 9 groups and 17 individual trees as part of the development. Of the trees to be felled, 5 groups and 14 trees are of poor quality and have low amenity value 77 trees would be planted as part of the landscaping proposals and 20 groups of trees and 54 trees would be retained on site. Given the high number of trees to be retained on the boundary of the site and the replacement trees, it is considered that the proposed development would not have an adverse impact upon the character and appearance of the area. Therefore, the proposed development would be in accordance with Policies EN8 and EN8/2 of the Bury Unitary Development Plan.

**Ecology** - An ecological survey and assessment were submitted as part of the application and additional surveys in relation to bats and nesting birds have been submitted. GM Ecology Unit has no objections, subject to the inclusion of conditions relating to tree protection measures, nesting birds, Japanese Knotweed and a habitat management plan and informatives relating to bats. Therefore, the proposed development would not cause harm to a protected species and would be in accordance with Policies EN6 and EN6/3 of the Bury Unitary Development Plan and the NPPF.

**Flood risk** - The application site is located within flood zone 1 and the standing advice from the Environment Agency classified residential development as 'more vulnerable'. It continues that a 'more vulnerable' development would be appropriate within this flood zone, providing that the site is not at risk of groundwater flooding. The applicant has submitted a drainage and a sustainable urban drainage (SuDS) strategy, which has identified measures to ensure that the site would not be at risk of groundwater flooding. United Utilities has no objections to the proposal, subject to a condition relating to SuDs and foul and surface water drainage. Therefore, the proposed development would not be at risk from groundwater flooding, subject to conditional control and would be in accordance with Policies EN5/1 and EN7/5 of the Bury Unitary Development Plan.

**Highways issues** - Access to the proposed development would be taken from Valley Park Road and Pinfold Drive and a Transport Assessment has been submitted as part of the application. A detailed assessment of the junction of Valley Park Road and Bury New Road has been completed and the junction would be able to accommodate the traffic generated. An emergency access would connect the proposed development to Kingswood Road.

There is a private right of access from Prestwich Hospital, along Thornton Crescent to Clifton Road. This right of access would be transferred onto the proposed access road and gates would be provided to prevent unauthorised access. The applicant has confirmed that all construction traffic would utilise Valley Park Road.

The Traffic Section has no objections in principle to the proposal. Further comments will be provided in the Supplementary Report.

**Parking** - SPD11 states that the maximum number of parking spaces is:

- 1.5 spaces per 2 bed
- 2 spaces per 3 bed
- 3 spaces per 4 and 5 bed units.

This equates to 261 parking spaces.

The proposed development would provide 178 spaces and 69 spaces in the form of garages, giving a total of 247 spaces. The site has good access to public transport and there would be space for visitors to park on the access road. As such, the level of parking provision in this instance would be acceptable. Therefore, the proposed development would be in accordance with Policy HT2/4 of the Bury Unitary Development Plan and SPD11.

**Planning Obligations** - the developer is providing recreational space on-site in accordance with Policy RT2/2 of the Bury Unitary Development Plan and SPD1. The recreational improvements include:

- woodland walkway;
- enhanced pedestrian paths;
- seating areas
- ecological water feature
- a 16 space car park to serve the existing playing fields.

The proposed development includes provisions for affordable housing within the site, comprising 24 affordable dwellings. SPG 5 states that the affordable housing units should reflect the house types on site and should be pepper potted around the site. The item was deferred from the last Planning Control Committee as the affordable dwellings were located in a single area of the site and did not reflect the mix of the dwellings. As such, the applicant has amended the layout of the proposed development and the mix of dwellings is as follows:

30 x 2 bed apartments;  
13 x 3 bed dwellings;  
54 x 4 bed dwellings.

The mix of affordable dwellings has been amended and would be as follows:

8 x 2 bed apartments  
4 x 3 bed dwellings  
12 x 4 bed dwelling.

The proposed mix of affordable dwellings would reflect the house types on site and the proposed dwellings would be pepper potted through the site. As such, the proposed development would be acceptable.

### **Response to objectors**

The issues relating to design, character of the area, scale of the development, loss of privacy and residential amenity, traffic, emergency access and its use, drainage and trees have been dealt with in the above report.

The proposed residential use would not lead to a significant increase in noise and as such, would not have an adverse impact upon the residential amenity of the neighbouring properties.

A condition has been included to restrict access for construction traffic to Valley Park Road and not through Clifton Road. Discussions have been held with Prestwich Hospital and it is proposed to construct a separate access for future developments at the site.

### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

**Recommendation:** Minded to Approve

### **Conditions/ Reasons**

1. The development must be begun not later than three years beginning with the date of this permission.  
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. This decision relates to drawings numbered Location plan, DSL-01 C, DSL -02 C, DSL-03 C, DSL-04 C, DSL-05 C, DSS-01, L4521APT2-901 D, L4521APT2-902 D, EF\_HARR\_DM.2.0 Rev A, EF\_BROD\_EM.1.0, EF\_OXFD\_DM.1.0, EF\_STRA\_DM.1.0, EF\_WELN\_DM.1.0 - elevations, EF\_WELN\_DM.1.0 - floor plan, EF\_CAMB\_DM.1.0, EF\_BROD\_EM.1.0 - floor plan, EF\_BROD\_EM.1.0 - elevations, EF\_WORC\_DM.1.0, EF\_CANT\_DM.2.0 - floor plan, EF\_CANT\_DM.2.0 - elevations, RED/LH/BCS01 B, C-DG03 1 001 B, C-SG01 1 001 B, ENG250-01 A, PRE/ENG001, PRE/ENG026, PRE/ENG039, PRE/ENG040, 4364/VSP-02, 4962.03  
4962.04 A, 4962.05 A and the development shall not be carried out except in accordance with the drawings hereby approved.  
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the (materials/bricks) to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.  
Reason. No material samples have been submitted and is required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
4. No development shall commence unless and until:-
  - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
  - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
  - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. Following the provisions of Condition 4 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.  
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
6. No development shall commence unless or until calculations to support the Drainage and SuDS Strategy has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved calculations and the submitted Flood Risk Assessment, Drainage and SuDS Strategy and Drainage Layout drawing.  
Reason. The scheme does not provide calculations to support the chosen design and to ensure a satisfactory means of drainage pursuant to Policy EN7/5 - Waste

## Water Management of the Bury Unitary Development Plan.

7. No development shall commence until full details of a scheme for the eradication and/or control of Japanese Knotweed (*Fallonia Japonica*, *Rouse Decraene*, *Polygonum Cuspidatum*) and Himalayan Balsam (*Impatiens Glandulifera*) is submitted to and approved in writing by the Local Planning Authority. The approved management plan shall include a timetable for implementation. Should a delay of more than one year occur between the date of approval of the management scheme and either the date of implementation of the management scheme or the date of development commencing, a further site survey must be undertaken and submitted to the Local Planning Authority.

Reason. The scheme does not provide full details of the actual extent of Japanese Knotweed and Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
8. Prior to the commencement of the development hereby approved, a five year habitat management plan shall be submitted to and approved in writing by the Local Planning Authority. The habitat management plan will include:

  - Mitigation for loss of trees, shrubs and the enhancement of the ground layer;
  - Details of any tree thinning works;
  - Provision of bird boxes;
  - Control of invasive species;
  - Enhance the ecological linkage with Mere Clough
  - A timetable for the proposed works

The approved plan shall be implemented in accordance with the approved timetable.

Reason. The scheme does not provide full details of the measures to preserve and enhance the habitat and features of ecological importance pursuant to Policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
9. No works shall be carried out to the trees that would disturb nesting birds between 1st March and 31st August inclusive in any year.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
10. All trees to be retained on site shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ until the development has been completed.

Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
11. No trees subject to a Tree Preservation Order, unless indicated otherwise on the approved plans, shall be felled, lopped or topped before, during or after the construction period.

Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
12. The landscaping scheme hereby approved shall be implemented to the written

satisfaction of the Local Planning Authority not later than 12 months from the date the building(s) is first occupied. Any trees or shrubs removed, dying or becoming severely damaged or becoming seriously diseased within 5 years of planting shall be replaced by trees or shrubs of a similar size and species to those originally required to be planted to the written satisfaction of the Local Planning Authority.  
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

13. Prior to first occupation of the development hereby approved, a detailed travel plan shall be submitted to and approved in writing by the Local Planning Authority. The approved travel plan shall be implemented in accordance with an agreed timetable.

Reason. In order to promote the use of sustainable modes of transport and minimise the use of the private car and single occupancy vehicles pursuant to the National Planning Policy Framework.

14. A minimum hardstanding of 5.5m shall be provided and thereafter maintained.  
Reason. To enable a vehicle to stand clear of the highway whilst the garage doors are opened and to allow adequate space to maintain a vehicle clear of the highway in the interests of road safety pursuant to Policy H2/2 - The Layout of New Residential Development of the Bury Unitary Development Plan.

15. Notwithstanding the details indicated on the approved plans, no development shall commence unless and until full details of the following have been submitted to and approved in writing by the Local Planning Authority:

- The configuration of the proposed turning head in the vicinity of Plots 77/80;
- The layout of the proposed junction adjacent to Plots 15/88;
- Revised driveway/access arrangements for Plots 96 and 97 to ensure that adequate forward visibility of vehicles entering and leaving the proposed driveways is provided;
- The layout and construction details of the proposed emergency access connection to Kingswood Road/West Road;
- The 20mph traffic calming scheme for the new residential development including details of proposed materials, road markings and signage;
- A programme for implementation of the works above;

The details subsequently approved shall be implemented in accordance with the approved programme.

Reason. The scheme does not provide details and to ensure good highway design and maintain the integrity of the adopted highway in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy EN1/2 - Townscape and Built Design

Policy H2/1 - The Form of New Residential Development

Policy H2/2 - The Layout of New Residential Development.

16. The development shall not be first occupied unless and until a scheme for the introduction of traffic regulation measures on the existing highway in the vicinity of the site access from Valley Park Road has been submitted to and approved in writing by the Local Planning Authority. The measures approved shall be implemented in accordance with an agreed timetable.

Reason. To mitigate the impact of the traffic generated by the proposed development on adjacent streets and to ensure that traffic travelling to and from the proposed development is not obstructed by parked vehicles, in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy EN1/2 - Townscape and Built Design

Policy H2/1 - The Form of New Residential Development

Policy H2/2 - The Layout of New Residential Development.

17. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and approved in writing by the Local Planning Authority. The approved plan shall be adhered to throughout the construction period and shall provide for:
- Access route for construction traffic from the adopted highway;
  - Hours of operation and number of vehicle movements;
  - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site.
- Reason. The scheme does not provide details of the route for construction and to mitigate the impact of the construction traffic generated by the proposed development on the adjacent streets, in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:  
Policy EN1/2 - Townscape and Built Design  
Policy H2/1 - The Form of New Residential Development  
Policy H2/2 - The Layout of New Residential Development.
18. The visibility splays, forward visibility envelopes and pedestrian visibility splays at the back edge of the footway indicated on the approved plans shall be implemented before the relevant parts of the development in which they are located are first occupied and subsequently maintained free of obstruction above the height of 0.6m.
- Reason. To ensure the intervisibility of the users of the site and the adjacent highways in the interests of road safety pursuant to the following Policies of the Bury Unitary Development Plan:  
Policy EN1/2 - Townscape and Built Design  
Policy H2/1 - The Form of New Residential Development  
Policy H2/2 - The Layout of New Residential Development.
19. The turning facilities on the proposed adopted highways and private driveways indicated on the approved plans shall be provided before the areas of the development to which they relate are first occupied and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times
- Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to the following Policies of the Bury Unitary Development Plan:  
Policy EN1/2 - Townscape and Built Design  
Policy H2/1 - The Form of New Residential Development  
Policy H2/2 - The Layout of New Residential Development.
20. The Developer, within one week of the commencement of development, shall ensure that there is provision to be made for the parking on site of operatives' and construction vehicles together with storage on site of construction materials. The provision shall be retained and used for the intended purpose for the duration of the construction period and the areas identified shall not be used for any other purposes other than the parking of vehicles and storage of construction materials respectively.
- Reason. To ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:  
Policy EN1/2 - Townscape and Built Design  
Policy H2/1 - The Form of New Residential Development  
Policy H2/2 - The Layout of New Residential Development.
21. Before the development is commenced, details shall be submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the

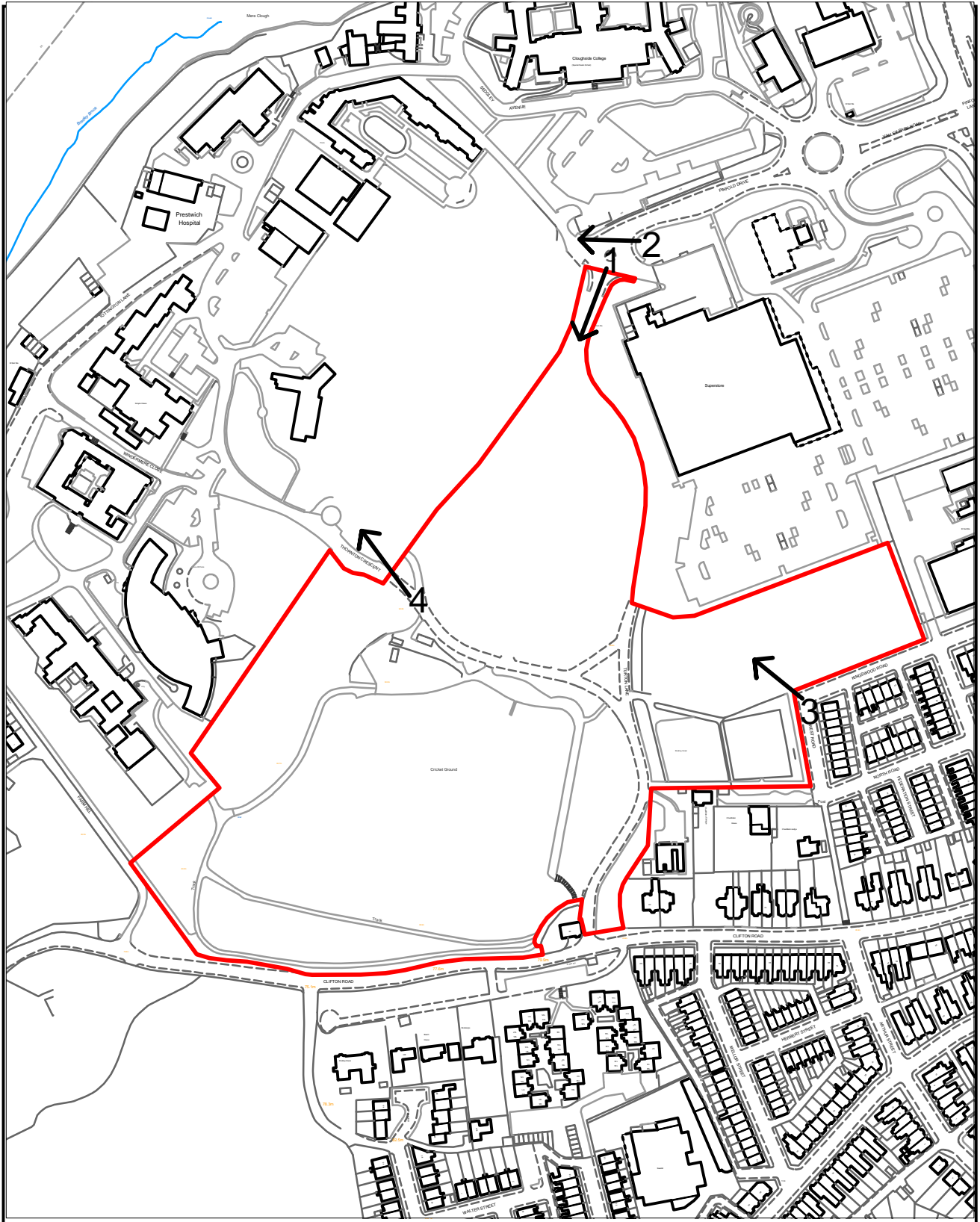
operations. The approved details shall be implemented and maintained thereafter during the period of construction.

Reason. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Policy H2/2 - The Layout of New Residential Development of the Bury Unitary Development Plan.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**



# Viewpoints



## PLANNING APPLICATION LOCATION PLAN

**APP. NO 58655**

**ADDRESS: Land at Valley Park Road  
Prestwich**



**Bury**  
COUNCIL

**Planning, Environmental and Regulatory Services 1:1250**

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58655

Photo 1



Photo 2





58655

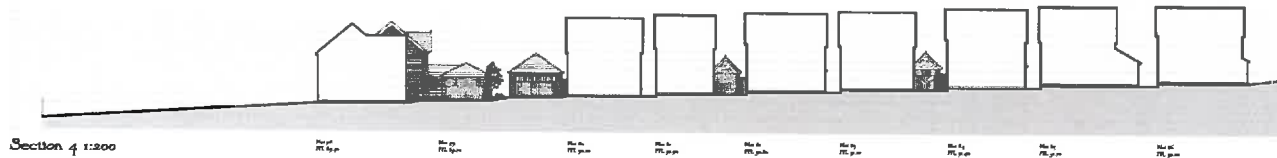
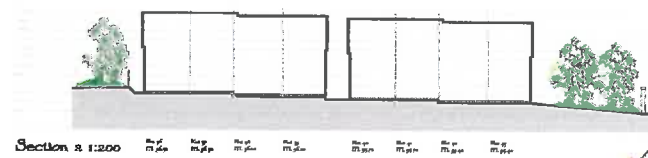
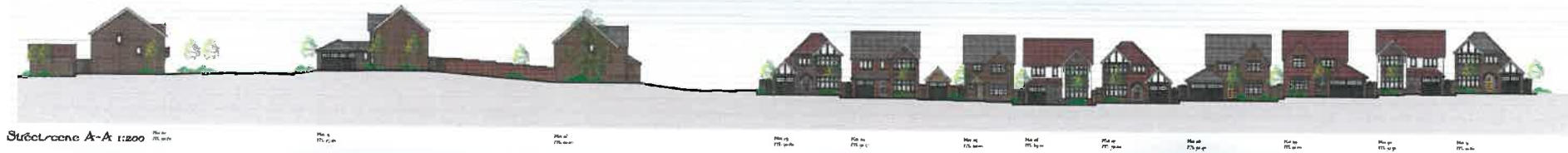
Photo 3



Photo 4



# Tesco, Prestwich



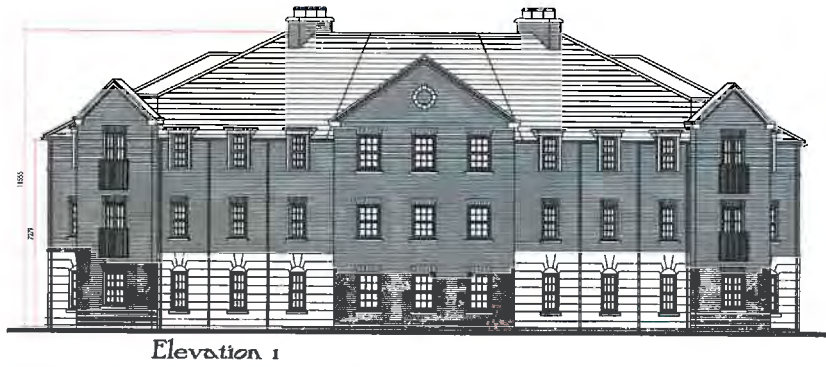
Site Plan

Project Name	Tesco Prestwich
Location	Prestwich
Client	ABC
Drawn by	Site Section and Detail section
Reference	DSS-01
Scale	1:200
Drawn by	CPM
Checked by	

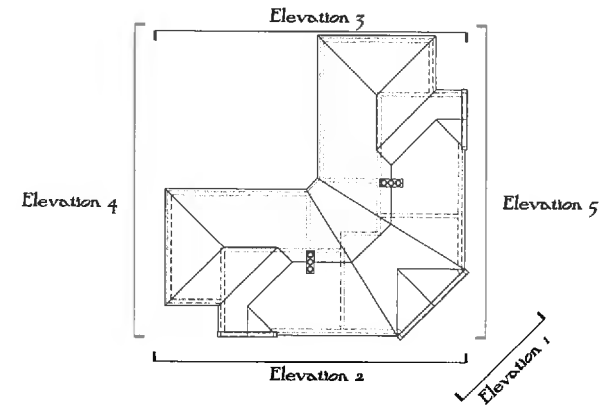
  

<b>REDROW</b> HOMES	
Redrow Homes Limited 1000 Oldham Road, Oldham, Greater Manchester, M6 9LQ Tel: 0161 275 2200 Fax: 0161 275 2201	
© Redrow Homes Ltd 2010	





Elevation 1



Elevation 2



Elevation 3



Elevation 4

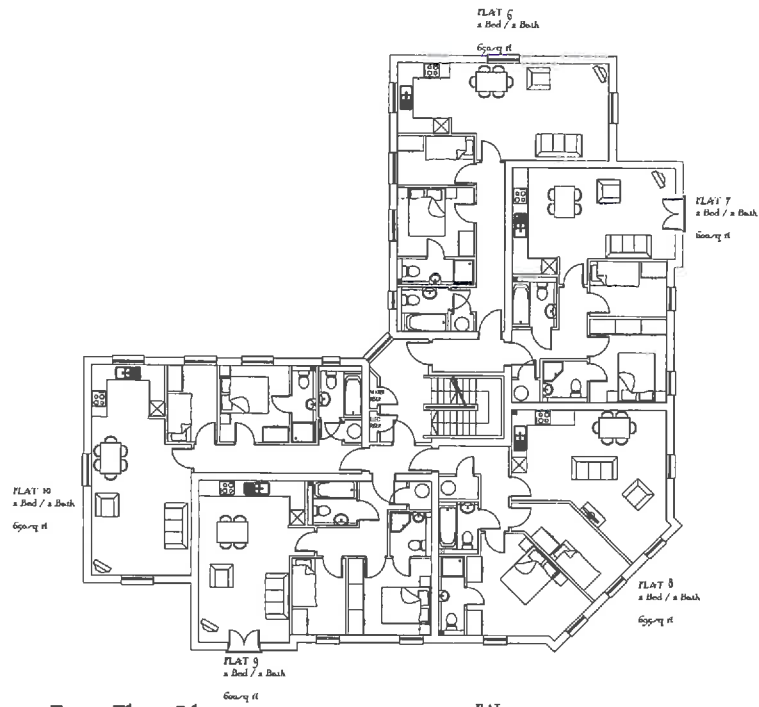


Elevation 5

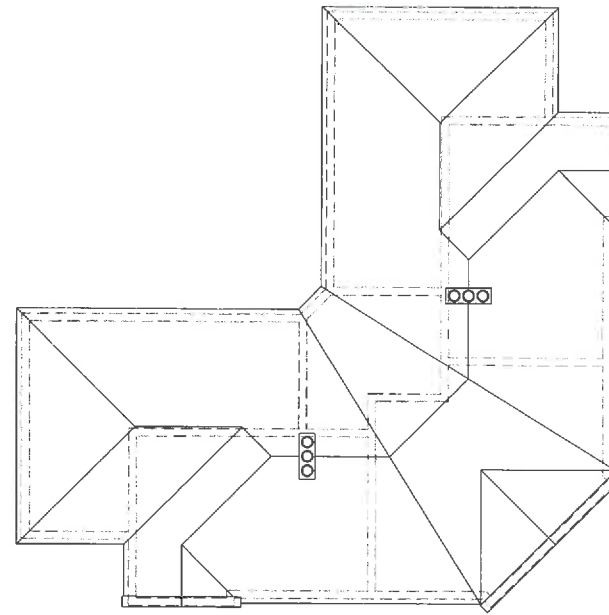
Prestwich  
D-Series  
PROPOSED ELEVATIONS

Dwg No. L4521APT2-902  
Date: Mar 2015  
REV: D  
Drawing Scale: 1/8"=1'-0"

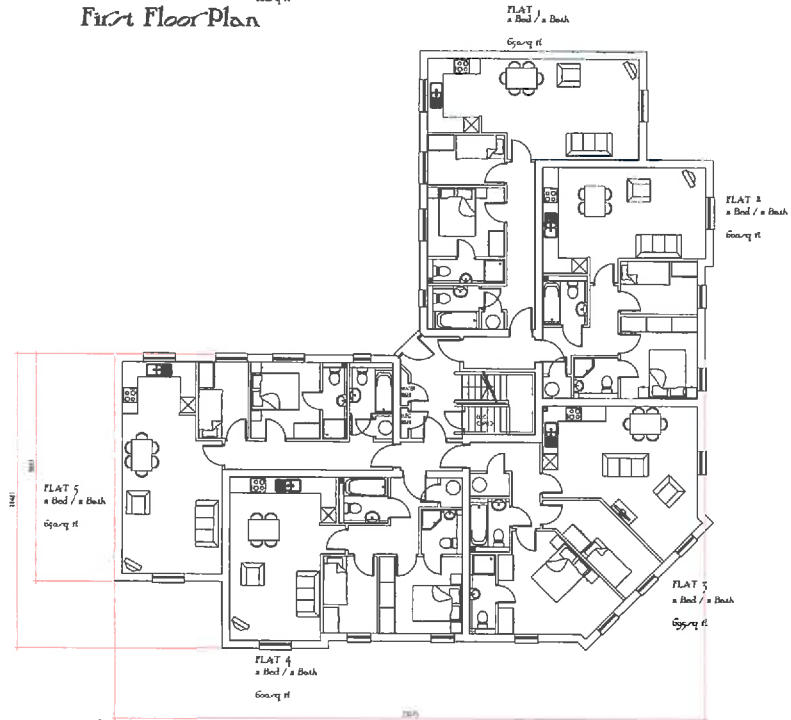




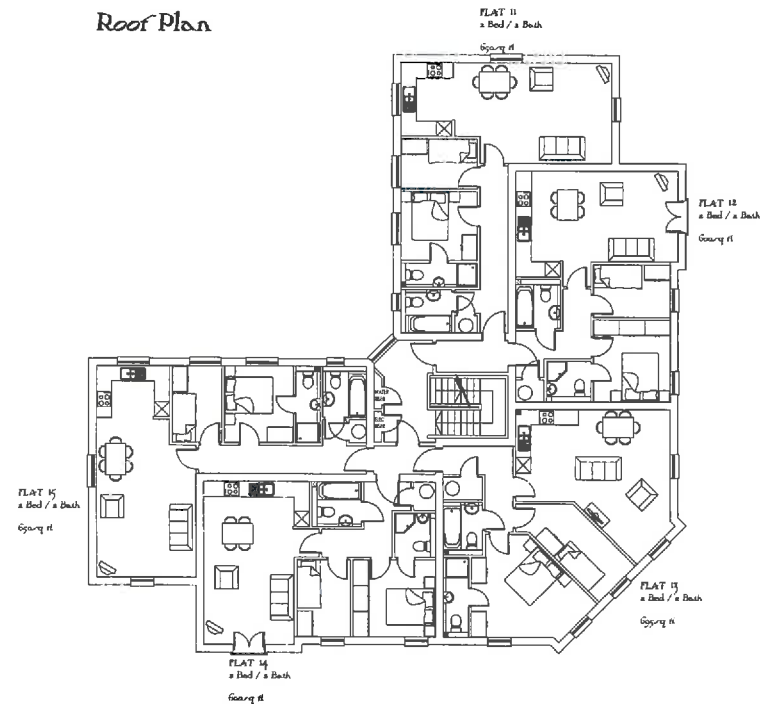
First Floor Plan



Roof Plan



Ground Floor Plan



Second Floor Plan

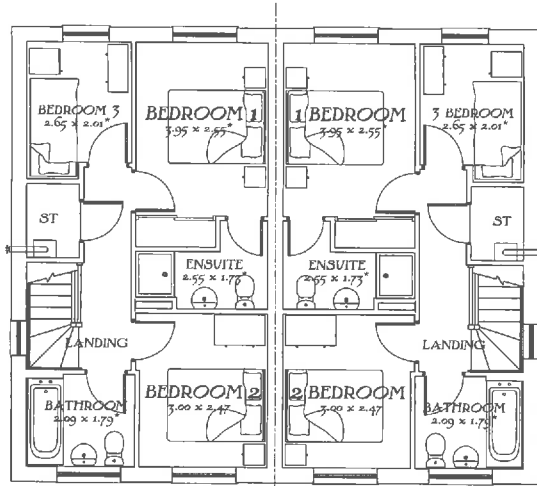
**Prestwich**

- D-Series
- 2 Bed FLAT 1, 5, 6, 10, 11 & 15  
650sqft 60.38m<sup>2</sup>
- 2 Bed FLAT 2, 4, 7, 9, 12 & 14  
600sqft 55.74m<sup>2</sup>
- 2 Bed FLAT 3, 8 & 13  
695sqft 64.56m<sup>2</sup>

**PROPOSED FLOOR PLANS**

Dwg No. L4521APT2-901  
Date: Mar 2012  
REV: D  
Drawing Scale: 1/1000

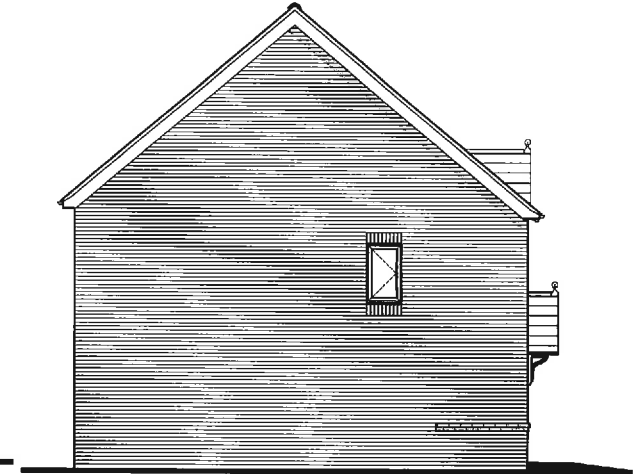




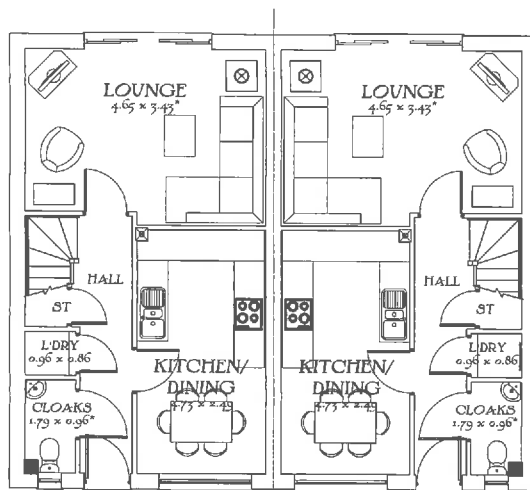
First Floor



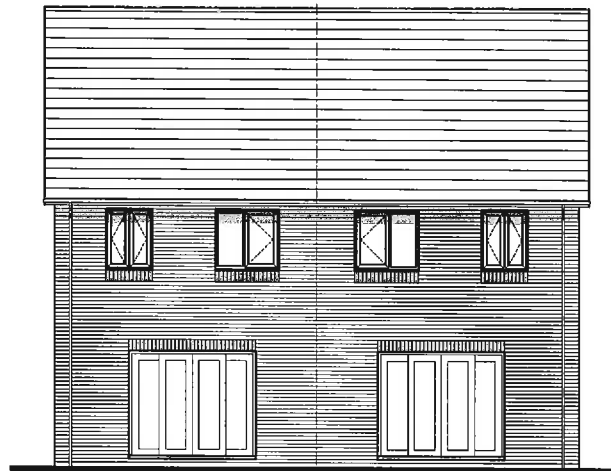
FRONT ELEVATION



SIDE ELEVATION



Ground Floor



REAR ELEVATION



The Broadway  
Heritage Collection  
"2014 Edition"

EF Series Semi-Detached (A1)

839sqft 77.94m<sup>2</sup>

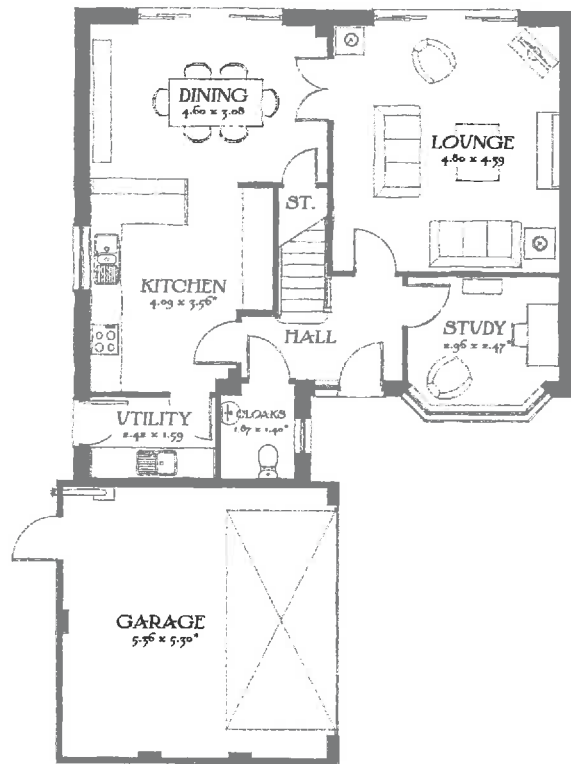
DATE: April 2014

REV:

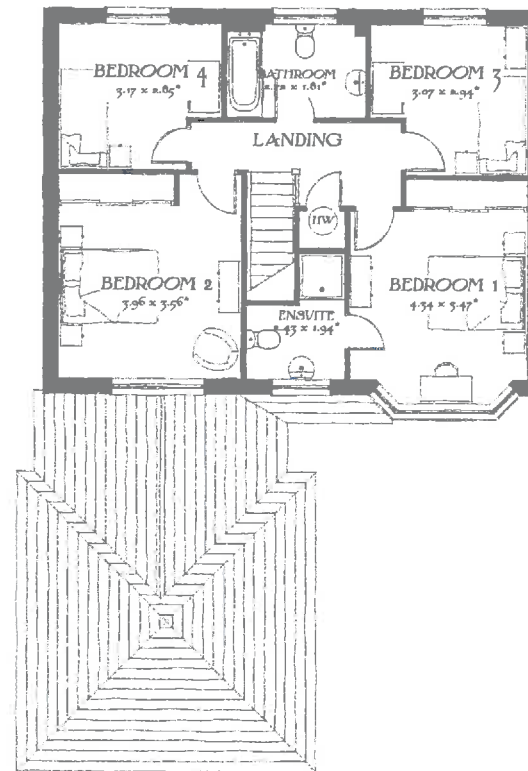
EF\_BROD\_EM.1.0

Drawing Scale 1:100 @ A3  
Bedroom dimensions taken into wardrobe recess  
\* Maximum Dimension

 **REDROW**  
GROUP SERVICES LTD  
www.redrow.co.uk



Ground Floor



First Floor



The Canterbury  
Heritage Collection  
"2014 Edition"

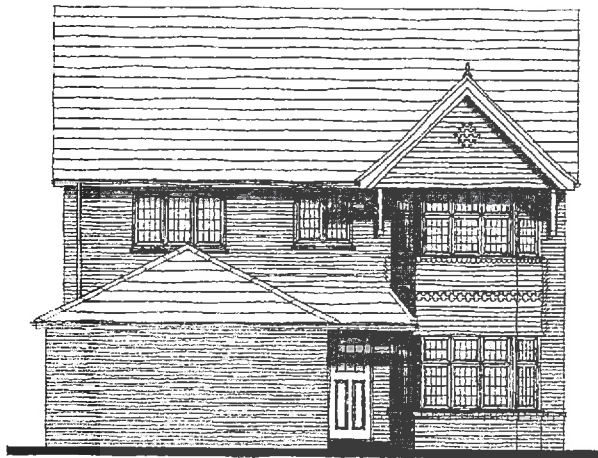
EF Series  
1482sqft 137.72m<sup>2</sup>  
DATE: March 2015  
REV: -

EF\_CANT\_DM.2.0

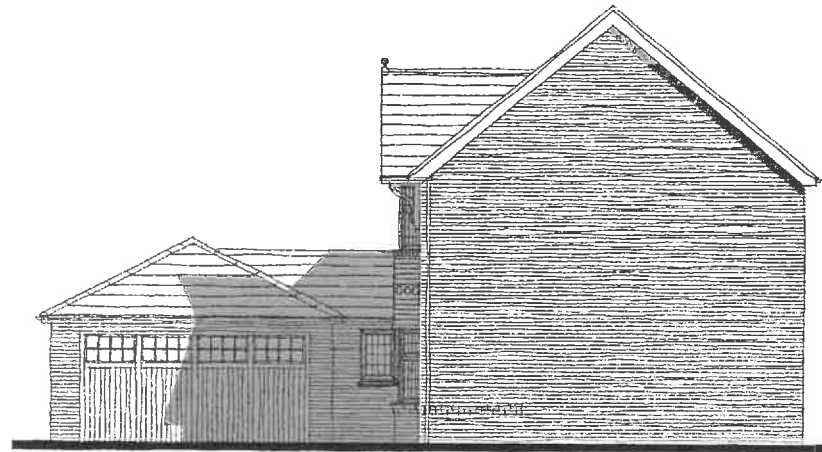
Drawing Scale 1:100 @ A3  
Bedroom dimensions taken into wardrobe recess  
\* Maximum Dimension



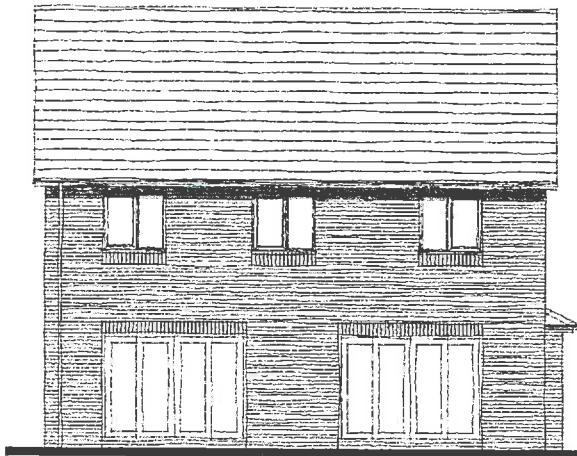




Front Elevation



Side Elevation



Rear Elevation



Side Elevation

## The Canterbury

Heritage Collection  
"2014 Edition"

EF Series Brick (B1)

1482sqft 137.72m<sup>2</sup>

DATE: March 2015

REV: -

EF\_CANT\_DM2.0

Drawing Scale 1:100 @ A3





Front Elevation



Side Elevation



Rear Elevation



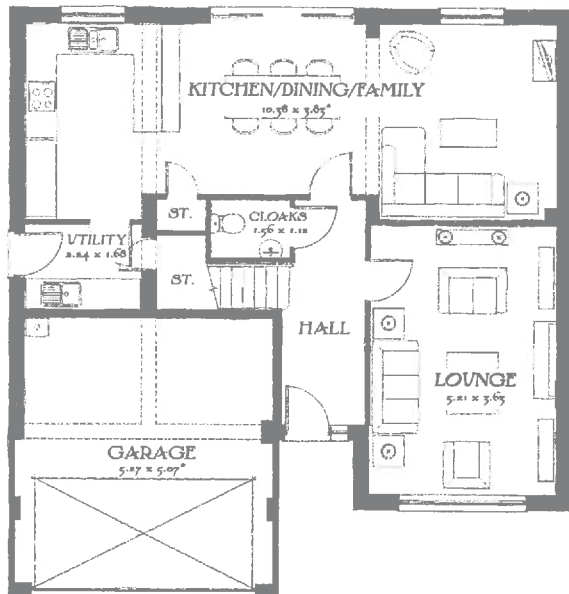
Side Elevation



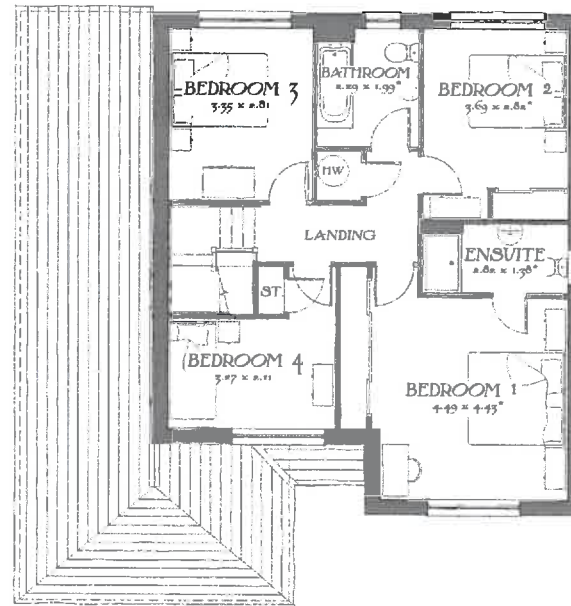
The Welwyn  
Heritage Collection  
"2014 Edition"

EF Series Render (A1)  
1533sqft 142.44m<sup>2</sup>  
DATE: April 2014  
REV:  
EF WELN DM.1.0

Drawing scale 1:100 @ A3  
Bedroom dimensions taken into wardrobe recess  
\* Maximum Dimension



Ground Floor



First Floor

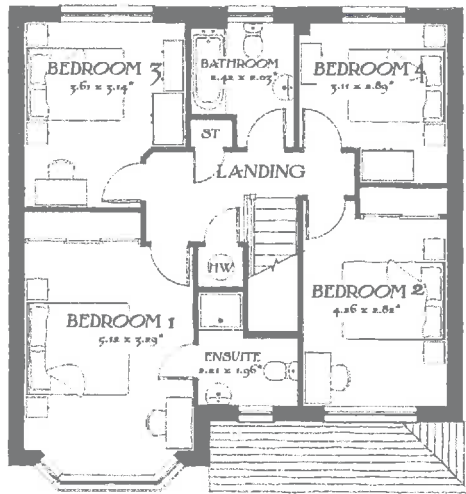


The Welwyn  
Heritage Collection  
"2014 Edition"

EF Series  
1533sqft 142.44m<sup>2</sup>  
DATE: April 2014  
REV:  
EF WBLN DM.1.0

Drawing scale 1:100 @ A3  
Bedroom dimensions taken into wardrobe recess  
\* Maximum Dimension

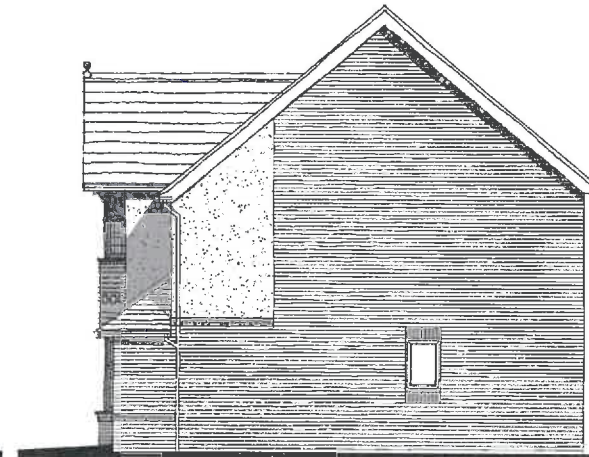




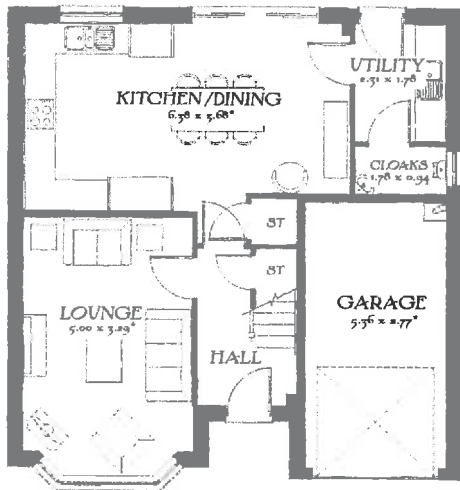
First Floor



Front Elevation



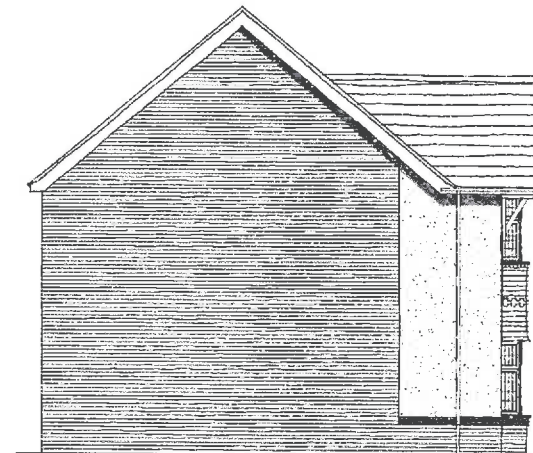
Side Elevation



Ground Floor



Rear Elevation

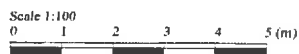


Side Elevation

The Oxford  
Heritage Collection  
"2014 Edition"

EF Series Render (A1)  
1301sqft 120.85m<sup>2</sup>  
DATE: April 2014  
REV: -  
EF OXFD DM.1.0

Drawing scale 1:100 @ A3  
Bedroom dimensions taken into wardrobe recess  
\* Maximum Dimension

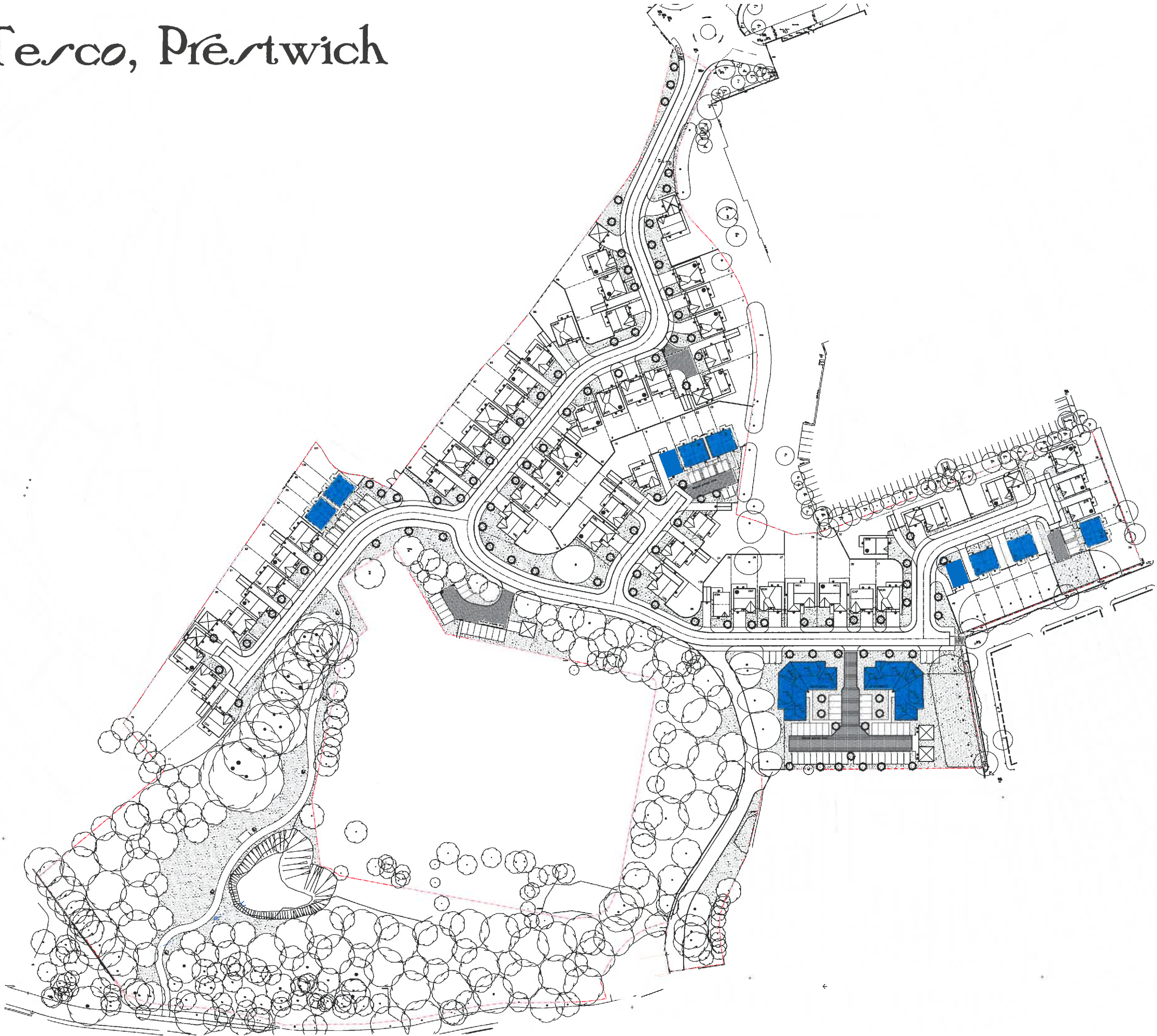









# Tesco, Prestwich



**Legend**

 Affordable Homes

Note: Affordable Units within both Apartment Block consist of 8 number affordable units, plots: 47, 48, 50, 51, 62, 63, 65 & 66

1	16.03.15	Issued for comment and review	W.C.
2	27.03.15	Approved for construction	A.C.
3	16.04.15	Issued for comment and review	W.C.
4	24.07.15	Approved for construction	A.C.
5	27.07.15	Issued for comment and review	W.C.

<b>Tesco Prestwich</b>	
Prestwich	
Client:	TSC
Project Name:	Affordable Homes Layout
Drawing Number:	DSL-06
Revision:	E
Scale:	1:500
Drawn by:	CPM
Checked by:	March 15

**REDROW HOMES**

Redrow Homes Lancashire  
 114-116, Victoria Road, Bolton, Greater Manchester, B7 4JG  
 Tel: 01204 642000 Fax: 01204 642001

Legal Disclaimer: No  
 This Report has been prepared under the supervision of a Chartered Land Surveyor

**Ward:** Bury West - Elton

Item 03

**Applicant:** St Vincents Housing Association &

**Location:** Land between 99 and 133 Crostons Road and Tottington Road, Bury, BL8 1AL

**Proposal:** Erection of 9 no. dwellings

**Application Ref:** 58805/Full

**Target Date:** 15/09/2015

**Recommendation:** Approve with Conditions

### **Description**

The site is located between Crostons Road, Tottington Road and Hulme Street and has planning permission for 34 residential apartments in two blocks. The block of apartments in the northern part of the site has been constructed and is occupied. The application site relates to the land to the south of Crostons Court, which is vacant land. The site slopes from west to east and there are advertisement hoardings on the boundary with Crostons Road. Vehicular access to the site is taken from the existing access on Tottington Road.

The site is surrounded by residential properties to the north, south and west. There are commercial properties, including car sales garage to the east of the site.

The proposed development involves the erection of 9 dwellings on land between Crostons Road and Tottington Road. The proposed dwellings would be two storeys in height and would be constructed from red brick with a tile roof. 5 of the dwellings would front onto Tottington Road and 4 would front onto Crostons Road. Vehicular access would be taken from Tottington Road and parking would be arranged centrally. The bin store for the proposed apartments would be relocated to the west and two visitor spaces would be provided.

### **Relevant Planning History**

43232 - Residential development - 34 No. flats at land between Tottington Road and Crostons Road, Bury. Approved with conditions - 30 June 2005.

52055 - Non material amendment to relocate bin store to service blocks 5 and 6 following grant of planning permission 43232 at Crostons Court, Bury. Approved - 17 December 2009.

52181 - Non material amendment to relocate bin store to service blocks 5 and 6 following grant of planning permission 43232 at Crostons Court, Bury. Approved - 17 February 2010.

57095 - Erection of 2 metre high paladin fencing to replace existing fencing and double leaf gate at land between Tottington Road and Crostons Road, Bury. Approved with conditions - 6 March 2014.

### **Publicity**

The neighbouring properties were notified by means of a letter on 16 June 2015 and a press notice was published in the Bury Times on 25 June 2015. Site notices were posted on 17 June 2015.

2 letters have been received from the occupiers of 99 Crostons Road, which has raised the following issues:

- Concerns about the boundary wall at the side and rear of 133 and 135 Crostons Road, which may be damaged.

- The builder should be required to repair/rebuild the wall.
- Concerned about the dwelling being constructed immediately adjacent to No. 99 Crostons Road.
- The proposed dwelling would block light to the windows in the gable elevation and would harm the outlook.
- The plans indicate a fence would be constructed along the full length of No. 99, which would belong to the neighbouring property. How can we gain access to our property for maintenance, repairs window cleaning etc?
- Concerned about damage to the property during construction.

The objectors have been notified of the Planning Control Committee meeting.

### **Consultations**

**Traffic Section** - No objections in principle and further comments will be reported in the Supplementary Report.

**Drainage Section** - Comments awaited and will be reported in the Supplementary Report.

**Environmental Health - Contaminated Land** - No objections, subject to the inclusion of conditions relating to contaminated land.

**Waste Management** - No objections, providing the bin store can accommodate 7 x 1100 litre bins as a minimum.

**Performance & Housing Strategy** - No response.

**Environment Agency** - No response.

**Design for security** - No response.

**United Utilities** - No objections, subject to the inclusion of conditions relating to foul and surface water drainage.

**Canal & River Trust** - No objections.

### **Unitary Development Plan and Policies**

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H4/1	Affordable Housing
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/7	Throughroutes and Gateways
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/5	Waste Water Management
RT2/2	Recreation Provision in New Housing Development
HT2/4	Car Parking and New Development
HT4	New Development
HT5/1	Access For Those with Special Needs
HT6/2	Pedestrian/Vehicular Conflict
SPD1	Open Space, Sport and Recreation Provision
SPD5	DC Policy Guidance Note 5: Affordable Housing
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
SPD16	Design and Layout of New Development in Bury
NPPF	National Planning Policy Framework

### **Issues and Analysis**

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be



specifically mentioned.

**Principle (residential)** - Following revocation of the North West Regional Strategy on 20 May 2013, there is no statutory housing target for Bury. Work has commenced on the Greater Manchester Spatial Framework and this will bring forward a new statutory housing target for the Borough. This will subsequently be incorporated into Bury's future Local Plan.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable housing land.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The site is located within the urban area and is in a residential area. As such, the proposed development would not conflict with the surrounding land uses and would be located in a sustainable location with good access to public transport and services. The site is partly developed in accordance with permission 43232, which was granted in June 2005 and as such, the site is previously developed land. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policy H1/2 of the Bury Unitary Development Plan and the NPPF.

**Design and layout** - The proposed development would utilise the existing access, with the proposed dwellings fronting onto both Crostons Road and Tottington Road. The provision of an active frontage on both roads is welcomed. The proposed dwellings would be two storeys in height and would be of traditional design. The height of the eaves of the proposed dwellings would match the eaves of the existing dwellings on Crostons Road. The provision of canopies, pike roof and brick banding to the headers and cills would add visual interest to the elevations.

The proposed dwellings would have a side or rear garden and would have an acceptable level of amenity space. There would be space within the rear or side gardens for bin storage. The bin store, which serves the existing apartments would be relocated to the west, which would allow two visitor spaces to be provided. The proposed boundary treatments would include a 1.8 metre high timber boarded fence, which would be acceptable and would match the existing fencing in the area. Therefore, the proposed development would be in accordance with Policies EN1/2, H2/1 and H2/2 of the Bury Unitary Development Plan.

**Impact upon residential amenity** - SPD6 provides guidance on aspect standards between residential properties and as such, would be relevant in this case.

There would be between 21 metres and 30 metres between plots 1 - 5 and 6 - 9, which would be in excess of the aspect standard. There would be between 15 metres and 20 metres between the front elevations of plots 1 - 5 and the land on Tottington Road and 10.2 metres between the gable of plot 5 and the gable of the existing apartments, which would be acceptable.

There is a sole window in the gable elevation to No. 99, which serves a room in the roofspace. There would be 2.5 metres between plot 9 and the existing window. There would be no openings within the gable elevation of plot 9 and the roof has been hipped. Given the above, it is considered that the proposed development would not have a significant adverse impact upon the amenity of the occupiers of the adjacent property.

Therefore, the proposed development would not have a significant adverse impact upon the amenity of the neighbouring properties and would be in accordance with Policies H2/1 and

H2/2 of the Bury Unitary Development Plan.

**Highways issues** - The proposed development would be accessed via the existing access on Tottington Road. There would be acceptable levels of visibility. The proposed layout has been amended and revised plans have been submitted by the applicant. The Traffic Section has no objections in principle and further comments will be reported in the supplementary report.

**Parking** - SPD11 states that the maximum number of parking spaces is 1.5 spaces per 2 bed unit and 2 spaces per 3 bed unit within a high access area. This equates to 16.5 spaces.

The proposed development would provide 18 spaces, which would include 3 visitor spaces. The proposed development is located in an area, where on-street parking could not take place and given the existing apartment block, it would be acceptable to provide 3 visitor spaces within the development in this instance. Therefore, the proposed development would be in accordance with Policy HT2/4 of the Bury Unitary Development Plan and SPD11.

**Planning Obligations** - The number of units has been reduced to 9 and as such, no contribution is required under Policy RT2/2 of the Bury Unitary Development Plan and SPD1 for recreation provision.

The scheme would fall below the trigger of SPD 5 to provide 25% affordable housing units. However, the scheme will deliver 100% much needed affordable housing in the Borough which is a clear benefit of the proposal.

#### **Response to objectors**

The issue of damage during construction and ongoing maintenance would be private matters and are not material planning considerations.

The issue of the impact upon residential amenity has been addressed within the main report.

#### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

**Recommendation:** Approve with Conditions

#### **Conditions/ Reasons**

1. The development must be begun not later than three years beginning with the date of this permission.  
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 1945-LOC, 1945-PL.EX01, 1945-PL.SP01, 1945-PL.SS01, 1945-HTB.01, 1945-PL.HTA.01, 1945-PL-D01 and the development shall not be carried out except in accordance with the drawings hereby approved.  
Reason. For the avoidance of doubt and to ensure a satisfactory standard of

design pursuant to the policies of the Bury Unitary Development Plan listed.

3. Details/Samples of the (materials/bricks) to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.

Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.

4. No development shall commence unless and until:-
  - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
  - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
  - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

5. Following the provisions of Condition 4 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

6. Any soil or soil forming materials brought to site for use in garden areas, soft landscaping, filling and level raising shall be tested for contamination and suitability for use on site. Proposals for contamination testing including testing schedules, sampling frequencies and allowable contaminant concentrations (as determined by appropriate risk assessment) and source material information shall be submitted to and approved in writing by the Local Planning Authority prior to any soil or soil forming materials being brought onto site, and; The approved contamination testing shall then be carried out and validatory evidence (soil descriptions, laboratory certificates, photographs etc) submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

7. All instances of contamination encountered during the development works which do not form part of an approved Remediation Strategy shall be reported to the Local Planning Authority (LPA) immediately and the following shall be carried out where appropriate:

- Any further investigation, risk assessment, remedial and / or protective works shall be carried out to agreed timescales and be approved by the LPA in writing;
- A Site Verification Report detailing the conclusions and actions taken at each stage of the works including validation works shall be submitted to, and approved in writing by, the LPA prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

8. No development shall commence unless or until details of foul and surface water drainage aspects, including an assessment of potential SuDS options for surface water drainage with calculations to support the chosen solution, have been submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented and be available for use before first occupation of the dwellings hereby approved.

Reason. To ensure a satisfactory means of drainage pursuant to Policy EN7/5 - Waste Water Management of the Bury Unitary Development Plan.

9. The landscaping scheme hereby approved shall be implemented to the written satisfaction of the Local Planning Authority not later than 12 months from the date the building(s) is first occupied. Any trees or shrubs removed, dying or becoming severely damaged or becoming seriously diseased within 5 years of planting shall be replaced by trees or shrubs of a similar size and species to those originally required to be planted to the written satisfaction of the Local Planning Authority.

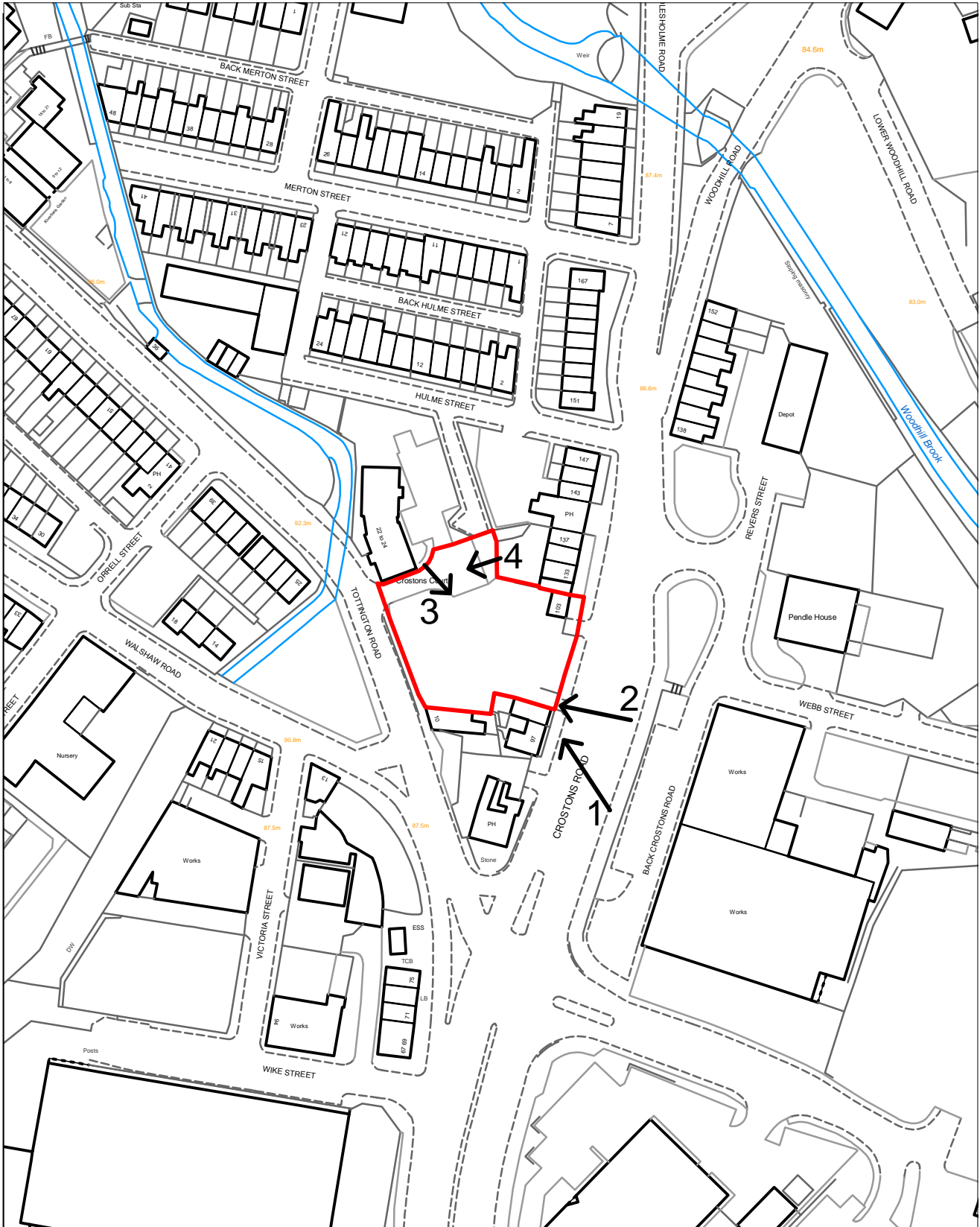
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

10. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) Order 1995, as subsequently amended, no development shall be carried out to plots 1 - 8 within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, without the prior written consent of the Local Planning Authority.

Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

# Viewpoints



## PLANNING APPLICATION LOCATION PLAN

APP. NO 58805

ADDRESS: Land at Crostons Road  
Bury

Planning, Environmental and Regulatory Services 1:1250

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58805

Photo 1



Photo 2



58805

Photo 3



Photo 4

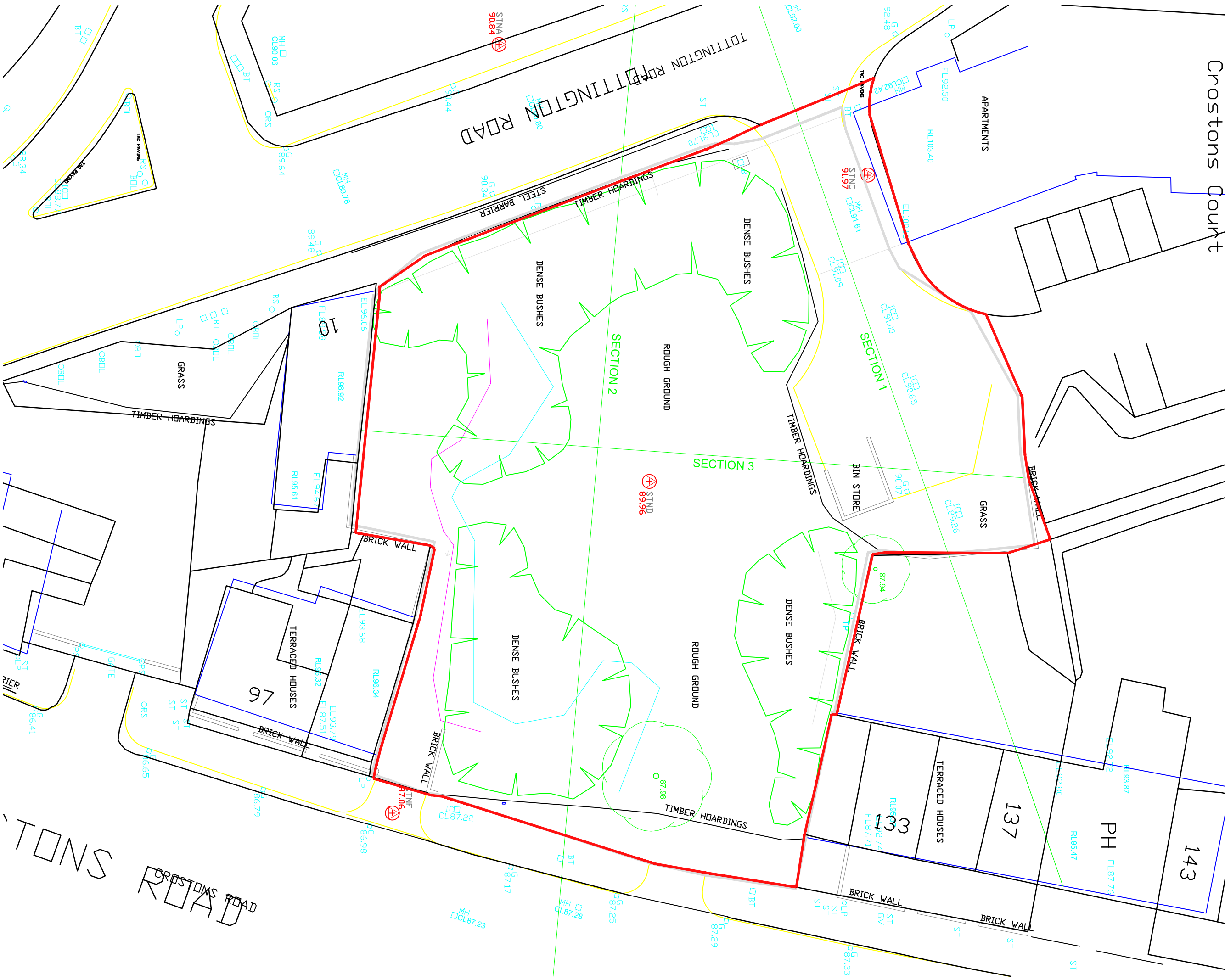




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Crosthons Court



		Client	
Date	20.05.15	Status	Planning
Drawn	SD	Checked	RB
Scale	1:250	Size	A3
Rev.	-	Drawing Nr.	1945-PL.EX01
Existing Site Plan		Drawing Title	
Proposed Redevelopment of Crosthons Rd / Tottington Rd, Bury.		Project Title	
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CROSTHONS ROAD

TOTTINGTON ROAD

APARTMENTS

GRASS

TERRACED HOUSES

DENSE BUSHES

TERRACED HOUSES

137

PH FL87.76

143

SECTION 2

SECTION 1

SECTION 3

STND 89.96

BIN STORE

GRASS

GRASS

DENSE BUSHES

DENSE BUSHES

ROUGH GROUND

ROUGH GROUND

ROUGH GROUND

TIMBER HOARDINGS

TIMBER HOARDINGS

TIMBER HOARDINGS

BRICK WALL

BRICK WALL

BRICK WALL

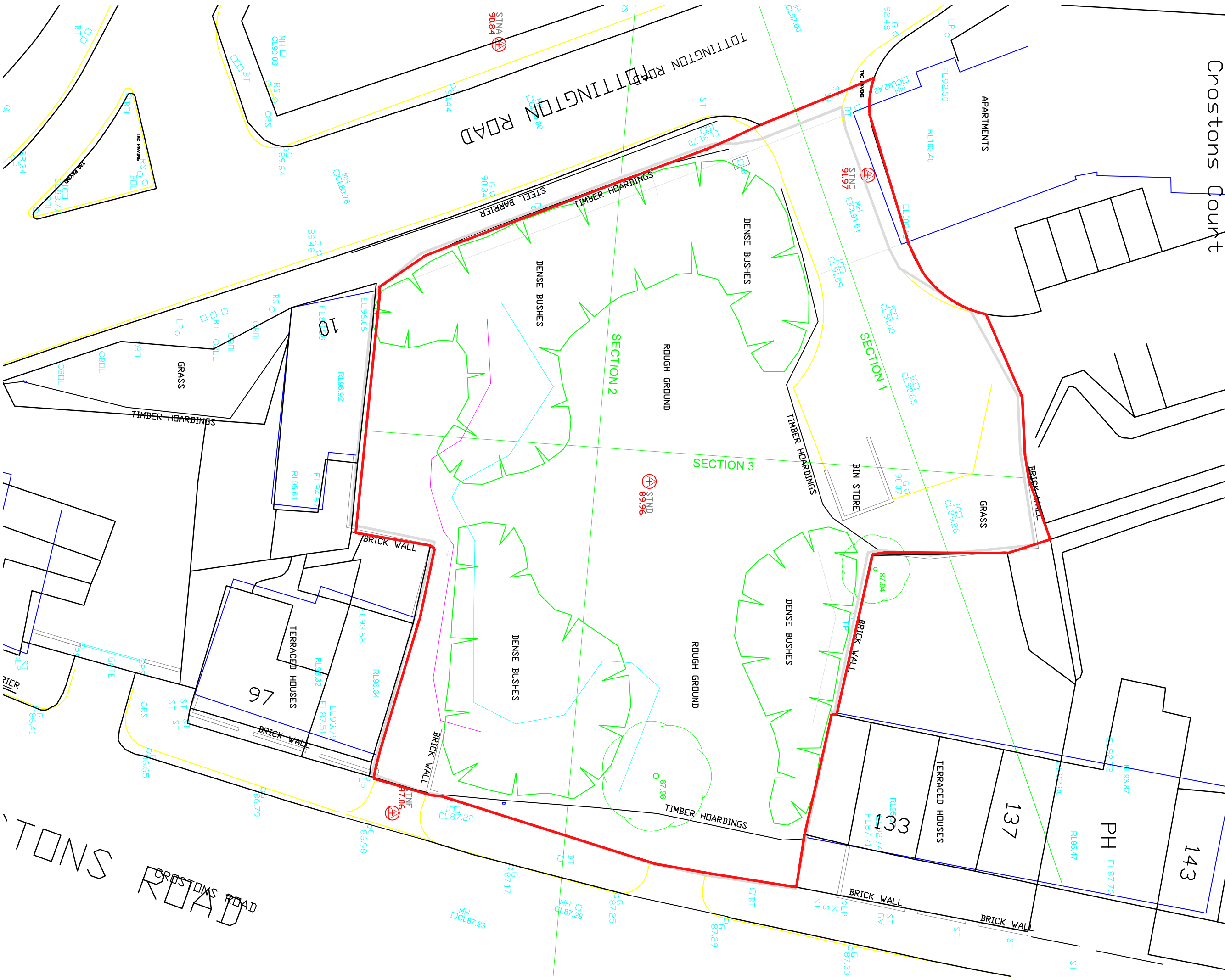
BRICK WALL

BRICK WALL

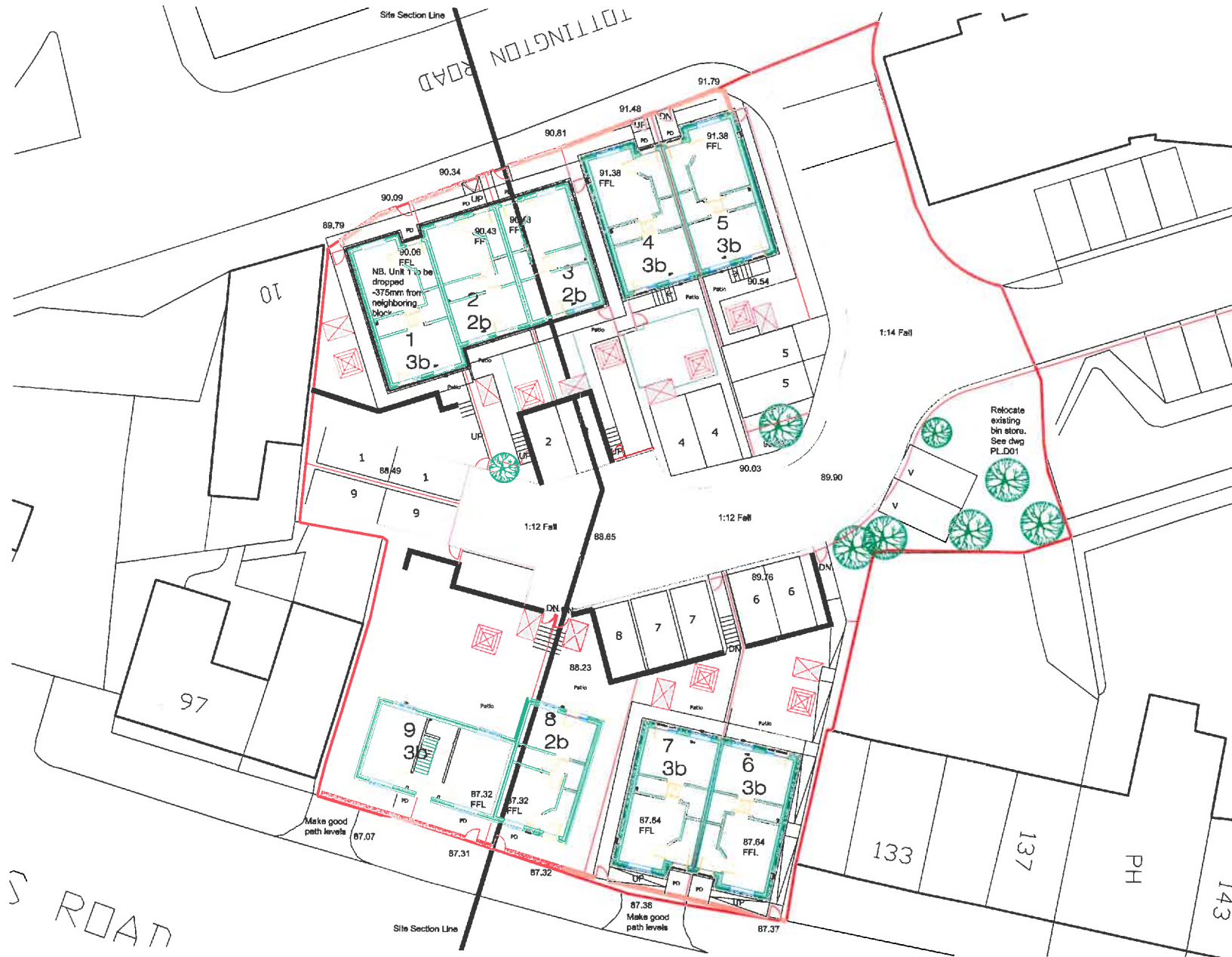
BRICK WALL

CROSTHONS ROAD

TOTTINGTON ROAD







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A | Plot 10 omitted

<b>bardsley</b>		Client
Date	20.05.15	Status Planning
Drawn	SD	Checked RB
Scale	1:250	Size A3
Rev.	-	Drawing Nr. 1945-PL.SP01
Drawing Title		
<b>Proposed Site Plan OPTION 1</b>		
Project Title		
<b>Proposed Redevelopment of Crostons Rd / Tottington Rd, Bury.</b>		
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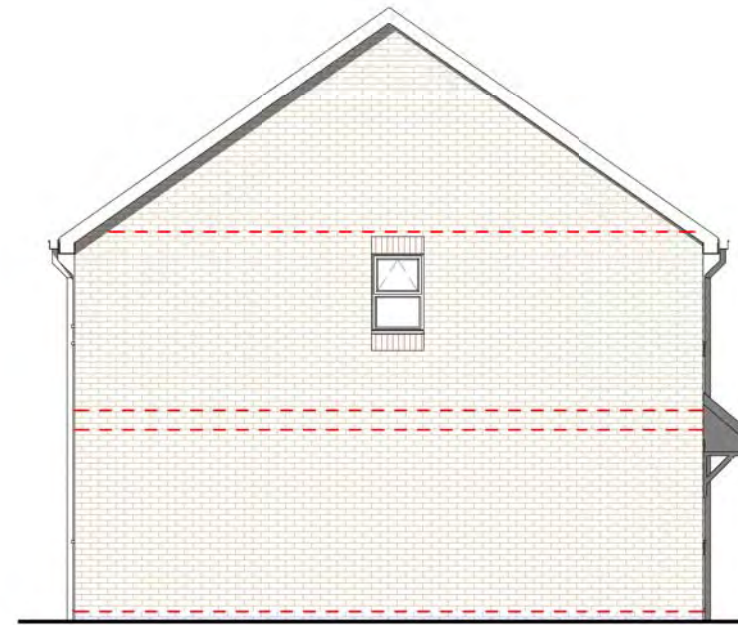
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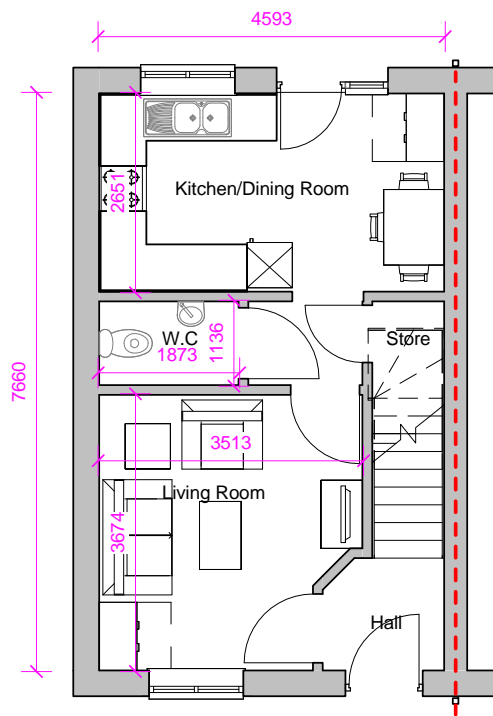
FRONT ELEVATION OVERVIEW  
1 : 100



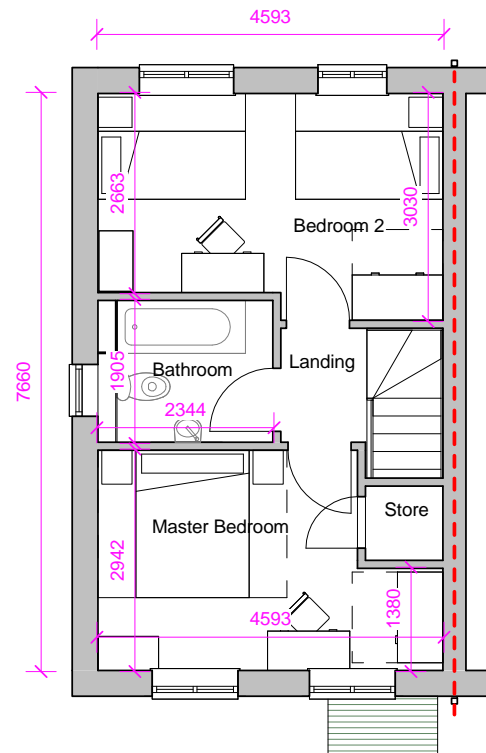
REAR ELEVATION OVERVIEW  
1 : 100



SIDE ELEVATION OVERVIEW  
1 : 100



GROUND FLOOR PLAN OVERVIEW  
1 : 100



FIRST FLOOR PLAN OVERVIEW  
1 : 100

### ROOM SCHEDULE (EXC. SERVICE VOIDS)

Name	Level	Area	Area Sq.Ft
Hall	GF	4 m <sup>2</sup>	40 ft <sup>2</sup>
Kitchen/Dining Room	GF	12 m <sup>2</sup>	129 ft <sup>2</sup>
Living Room	GF	12 m <sup>2</sup>	127 ft <sup>2</sup>
Store	GF	2 m <sup>2</sup>	18 ft <sup>2</sup>
W.C	GF	2 m <sup>2</sup>	22 ft <sup>2</sup>
		31 m <sup>2</sup>	335 ft <sup>2</sup>
Bathroom	FST	4 m <sup>2</sup>	45 ft <sup>2</sup>
Bedroom 2	FST	13 m <sup>2</sup>	138 ft <sup>2</sup>
Landing	FST	4 m <sup>2</sup>	40 ft <sup>2</sup>
Master Bedroom	FST	12 m <sup>2</sup>	124 ft <sup>2</sup>
		32 m <sup>2</sup>	347 ft <sup>2</sup>
		63 m <sup>2</sup>	682 ft <sup>2</sup>

### AREA SCHEDULE (Gross Internal Area)

Area reference	Level	Area Sq.m	Area Sq.ft
Ground Floor	GF	35.180 m <sup>2</sup>	378.675 ft <sup>2</sup>
First Floor	FST	35.180 m <sup>2</sup>	378.675 ft <sup>2</sup>
		70.360 m <sup>2</sup>	757.349 ft <sup>2</sup>

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W: <http://www.bardsley.co.uk>

**bardsley**

Client

Drawing Title  
**2 Bed House Type  
Plans & Elevations Overview**

Date 20/05/15 Status Preliminary  
Drawn SD Checked RB  
Scale 1 : 100 Size A3

Project Title  
Tottington Road

Revision Drawing Number  
1945-PL.HTA.01

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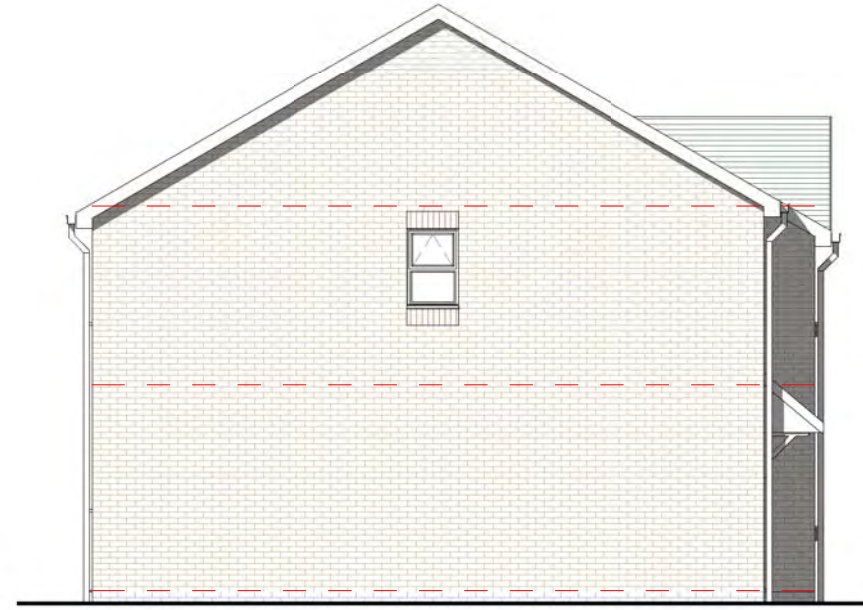




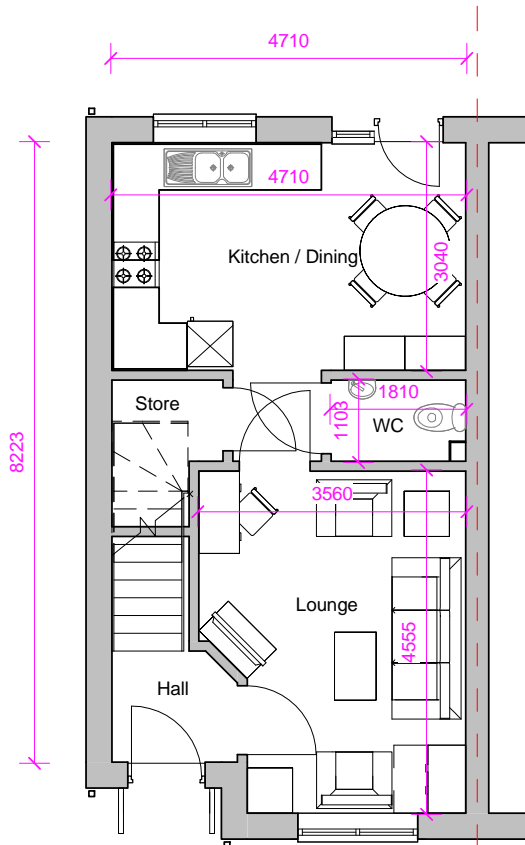
FRONT ELEVATION OVERVIEW  
1 : 100



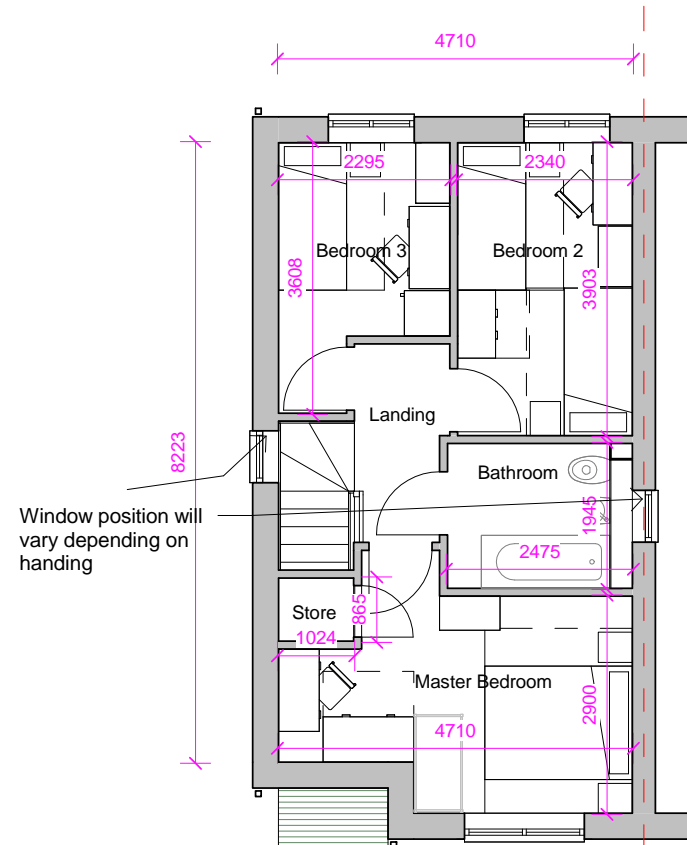
REAR ELEVATION OVERVIEW  
1 : 100



SIDE ELEVATION OVERVIEW  
1 : 100



GROUND FLOOR PLAN OVERVIEW  
1 : 100



FIRST FLOOR PLAN OVERVIEW  
1 : 100

**ROOM SCHEDULE (EXC. SERVICE VOIDS)**

Name	Level	Area	Area Sq.Ft
Lounge	GF	15 m <sup>2</sup>	158 ft <sup>2</sup>
WC	GF	2 m <sup>2</sup>	20 ft <sup>2</sup>
Kitchen / Dining	GF	15 m <sup>2</sup>	167 ft <sup>2</sup>
Hall	GF	4 m <sup>2</sup>	42 ft <sup>2</sup>
Store	GF	2 m <sup>2</sup>	26 ft <sup>2</sup>
		38 m <sup>2</sup>	413 ft <sup>2</sup>
Master Bedroom	FST	12 m <sup>2</sup>	130 ft <sup>2</sup>
Bedroom 3	FST	7 m <sup>2</sup>	72 ft <sup>2</sup>
Bedroom 2	FST	9 m <sup>2</sup>	96 ft <sup>2</sup>
Bathroom	FST	5 m <sup>2</sup>	50 ft <sup>2</sup>
Landing	FST	5 m <sup>2</sup>	55 ft <sup>2</sup>
		37 m <sup>2</sup>	403 ft <sup>2</sup>
		76 m <sup>2</sup>	816 ft <sup>2</sup>

**AREA SCHEDULE (Gross Internal Area)**

Area reference	Level	Area Sq.m	Area Sq.ft
Ground Floor	GF	40.699 m <sup>2</sup>	438.080 ft <sup>2</sup>
First Floor	FST	40.699 m <sup>2</sup>	438.080 ft <sup>2</sup>
		81.398 m <sup>2</sup>	876.160 ft <sup>2</sup>

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Client

Drawing Title  
3 Bed House Type  
Plans & Elevations Overview

Project Title  
Tottington Road

Date 20/05/15 Status Planning

Drawn SD Checked RB

Scale 1 : 100 Size A3

Revision

Drawing Number  
1945 - HTB.01

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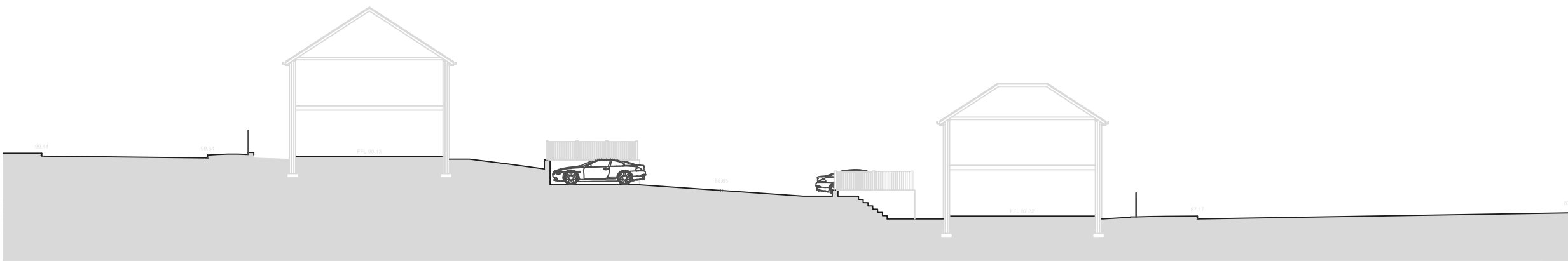
# Street scene elevation of Crostons Road 1:100

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- -



Site Section 1:250

		Client	
Date	20.05.15	Status	Planning
Drawn	SD	Checked	RB
Scale	1:250 / 1:100	Size	A3
Rev.	-	Drawing Nr.	1945-PL.SS01
Drawing Title			
Proposed Street Scene to Crostons Road & Site Section			
Project Title			
Proposed Redevelopment of Crostons Rd / Tottington Rd, Bury.			
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**Ward:** Whitefield + Unsworth - Pilkington Park

Item 04

**Applicant:** Whitefield Golf Club

**Location:** Whitefield Golf Club, Higher Lane, Whitefield, Manchester, M45 7EZ

**Proposal:** Erection of 1 no. 30M (hub) high, 45m (tip) high wind turbine plus ancillary development

**Application Ref:** 58874/Full

**Target Date:** 25/08/2015

**Recommendation:** Refuse

**The item has been referred to the Planning Control Committee due to being a novel planning matter following the publication of the Ministerial Statement of 18th June 2015 by the Secretary of State, Greg Clark, for Communities and Local Government.**

**The Development Manager has recommended a site visit take place before the Planning Control Committee meeting.**

### **Description**

The application site relates to land which is located within the southern area of the grounds of Whitefield Golf Club, designated as Green Belt land as defined in the Bury Unitary Development Plan. The topography of the site is relatively level, although there are areas of undulating land. Natural vegetation and tree belts are found around the periphery and within the site that screen certain areas of the golf course.

The wider area surrounding the golf club is predominantly residential in character. The site is bounded by Higher Lane to the north, Ross Avenue to the east, and Park Lane and Sedgley Rugby Club to the west. To the south is Philips Park Road, a pedestrian path and track which runs parallel to the M60 motorway. Beyond this is Philips Park, which itself is not a listed park or garden, but contains The Gate Lodge, the Conservatory and the Garden Temple, all of which are listed.

Proposal - The application is for the erection of a wind turbine. The hub height would be 30m and the rotor blade diameter 30m, equating to a total height of 45m. It would have 3 rotational blades. The hub would be made of cast steel, the main shaft a forged piece of alloyed steel painted in light grey with a matt finish, and the blades glassfibre with 'fail safe' tip brakes.

Specification - The turbine would be a Wind Technik Nord 250kW. It would have a tubular tower and be equipped with a disk brake system and control panel linked to a computer management system. A technical specification has been provided at Appendix 2 of the application. The rotor speed would be 40 rpm. The estimated amount of energy which would be generated would equate to powering 134 homes per annum. The energy generated would be sold to the National Grid.

Siting - The turbine would be located in the southern part of the golf club grounds. It would be approximately 50m north of Philips Park Road and approximately 72m to the M60 motorway boundary. The nearest residential properties would be equi distant 284m away to the east on Westlands, and to the west on Park Lane.

There is a network of public footpaths towards the southern part of the golf course, which connect to areas outside the site, and which would require either diversion or closure orders to facilitate the siting of the turbine.

Access - The turbine would be delivered to the site via Philips Park Road off Park Lane and would require three 45 foot long articulated vehicles. Two cranes would be required to install the turbine and these would be delivered using the same route.

The location of the site would be such that a temporary access road would need to be created from Philips Park Road. It would be 3.5m wide, approximately 60m in length and constructed of a hardcore MOT overlain on a protective mat. The ground would be returned to its former state once the installation is completed.

### **Relevant Planning History**

43773 - Outline - demolition of existing clubhouse & stores & construction of replacement golf clubhouse, car parking & new access to higher lane, together with erection of 24 no. apartments - Approve with Conditions 17/03/2005

44276 - reserved matters - demolition of existing clubhouse & stores & construction of replacement golf clubhouse, car parking & new access to higher lane, together with erection of 24 no. two bedroom apartments - Approve with Conditions 31/05/2005

45680 - Non illuminated free standing advertisement sign - Approve with Conditions 06/03/2006

47058 - Reserved matters : variation of apartment buildings and design amendments to 44276 application approved for 24 apartments and associated basement and surface car parking and landscaping - Approve with Conditions 29/03/2007

47163 - Omission of acoustic fence adjacent to the boundary and included on the approval for the new golf club application number 43773 (variance/removal of condition 10 from planning permission 43773) & increase height of the boundary wall to 3.0m high (taken from 71/73 Higher Lane) - Approve with Conditions 08/02/2007

48139 - Replace damaged and defective fencing, providing new security fencing to the boundary adjacent to Ross avenue and top o' th' fields (resubmission) - Approve with Conditions 17/07/2007

50494 - Toilet extension to existing refreshment hut adjacent 10th tee (retrospective) with new pitched roof to building (resubmission) - Approve with Conditions 26/11/2008

55406 - Erection of three bay junior golf practice area - Approve with Conditions 17/10/2012

56095 - Single storey extension under existing balcony at front of clubhouse. - Approve with Conditions 15/04/2013

56178 - Non-material amendment following grant of planning permission 55406 for the erection of three bay junior golf practice area to increase the bay sizes - Approve with Conditions 30/04/2013

### **Publicity**

625 neighbour notification letters sent on 2/7/2015 and 8/7/2015. A full list of those notified is available on the public file.

Site notices placed in the vicinity 7/8/2015.

Press advert published in the Bury Times on 9/7/2015.

Objections - 148

Support - 5 petitions with a total of 68 signatures and 16 individual letters.

Objections - The following issues have been raised by the objectors. Please note - these are summaries only. The full details are available to view on the public file.

- Noise pollution and sleep disruption;
- Would encourage other turbines in the area;
- Reduce house values;
- Already put up with rugby club floodlights, ugly communications mast and the 8 lane motorway - without having the skyline blighted by a huge turbine;
- Cannot believe the electricity produced is solely for golf club - if profit making why can't not solar panels or couple of smaller turbines situated in the far corners of the course away from residential properties - less obtrusive and more income generated;

- Very little electricity produced to help benefit locals - only the golf club;
- Loss of visual amenity;
- Visual strobing; Can cause epileptic fits;
- Financial impacts on business as people would avoid the area due to health implications;
- Inappropriate in a residential area;
- Affect on the surrounding historic buildings;
- Skyline dramatically changed for the worse;
- Should focus on the failed building site at the front - an eyesore;
- Would the Planning Committee like a structure like this at the back of their homes annoyingly whooshing away night and day;
- Concerned about health implications; Request a Health Impact assessment
- safety implications if it collapsed close to a busy motorway;
- Close to a network of public rights of way - will require diversions and possibly closures;
- Invasion of Green Belt land; A duty to protect Green Belt;
- Damage to trees, shrubs and plants in the vicinity;
- Negative effect on wildlife;
- sparse leaflet drop to residents;
- Proposed turbine is second hand model - so not incorporate latest technology and have a shorter lifespan;
- Re-surfacing of motorway would reduce noise levels - therefore a nonsense of the present noise assessment;
- Damage on installation - disruption to the local area and unadopted roads;
- The golf club claim it is needed to save them from liquidation - they have to work with local residents and should look for more acceptable means of raising money;
- In case of an accidental fire, houses nearby would not escape damage;
- The community fund which would be set up by the golf club would not benefit the local community
- Distraction to drivers on the motorway
- Removal of the bollards on Phillips Park road may see the return of fly tipping;
- may cause further problems regarding location of Agecroft Colliery mine workings;
- Disrupt the peace and amenity of the area;
- Golf club did not disclose to the community the sale of land adjoining the turbine site to Sedgley Park Rugby club - demonstrates lack of community involvement - affects Green belt land;
- Contrary to the NPPF; Not demonstrated Very special circumstances;
- Misleading information of the noise report;
- Info submitted purports to a turbine at Staffordshire Moorlands District Council - has been confirmed the generating capacity is not applicable to the Whitefield location;
- Viability of the project is seriously doubtful;
- Shadow flicker;
- On 18 June 2015 the DCLG issued guidelines relating to planning applications - stated that new rules mean wind turbines should only get the go-ahead if they have been clearly backed by local people - In view of substantial opposition, it should not be granted approval;
- Scaremongering by the golf club to deter local residents from objecting;
- Low frequency noise from turbines causes health problems;
- Why are the leisure needs of the golf club more important to the council than the daily lives of the local residents:
- Why erect a turbine at all when the Secretary of State for Energy and Climate Change (Rt Hon Amber Rudd MP) recently said of onshore wind turbines that "we now have enough projects in the pipeline to meet our renewable energy commitments.?"
- Affect the surrounding historic buildings;
- Would the planning committee like a structure like this at the back of their homes annoyingly whooshing away night and day;
- The proposed location of the turbine places it close to a network of public rights of way at the southern edge of the golf course. Given that the turbine is 45m in height, the



turbine needs to be 45m +10% away from the nearest public right of way;

- Some rationalisation of the public right of way network at this location will be required before this development can go ahead and this will require diversions and possibly closures;
- Aviation Hazard to City Airport and Heliport at Barton, Microlight aircraft, various flying schools, hot air balloonists and helicopters which all use the air space above the turbine proposed area;
- There are young boys and girls that play and train at Sedgley RUFC, Park Lane, most weekends and the proposed site is alongside the rugby ground. What health risks could there be for these boys and girls with the noise and strobing from this wind turbine?;
- Impact on the public footpaths in the area;
- Poor and lack of community consultation by the Golf Club;
- Danger to hot air balloonists, microlights and helicopters;
- Phillips Park is Bury's first Local Nature Reserve and a Site of Biological Importance;
- Impact on Grade II Listed entrance to Phillips park;
- Reduced Government subsidies is questionable to the Club making profits;
- Landscape and Visual impact Assessment are poor quality and photomontages taken from non-vantage points;
- Turbine plan not shown to scale;
- Replacing the Hutchinson Mast with the turbine that is a net benefit when assessing the application is ludicrous statement;
- New scientific discovery made proving much lower frequency auditory responses to noise;
- Claims of profit are overstated by the Club;
- Scientific papers prove turbines have negative impact on health;

Objection from a resident in Affetside -

- Too close to residential properties, being 280m from the nearest properties, the next nearest being 310m and 360m away. Inevitable problems with noise and flicker;
- Draw your attention to a planning Appeal for a turbine on another golf course in Bolton, dismissed 2012, one of the reasons being implications for neighbour's living conditions with reference to outlook and disturbance from noise and shadow flicker;
- This application fails to comply with UDP policy EN4/1;
- Inappropriate development in the Green Belt;
- Contrary to the NPPF - Very Special Circumstances not demonstrated.

Objection from BRIF -

- Unacceptable visual impact;
- Unreasonable location on designated Green belt;
- Noise disturbance to local residents;

Objection received from resident in Affetside which questions the technical qualities of the turbine, its installation and performance, and this misnomer and flaws of the submitted planning application.

Objections received from the "saynotowindinthewillows" group, representing the Whitefield Residents' Action Group, raising the following issues:

- Believe the claims made in the consultee comments from GMEU to be incorrect and challengeable;
- The Council must adhere to Government Directives about proposals which would damage the Green Belt, or, specific to wind turbines, that does not have the backing of the local community;
- Overwhelming that the number of objectors and expert objections does not emphatically have the backing of the community;
- The golf club have no regard for Greenbelt land, no regard for residential amenity, no regard for loss of wildlife or discretion of historic areas;

- Rumours that land has been sold to the rugby club for future development must be taken seriously;
- Advise that the Golf Club have completely obliterated the ancient network of public rights of way;
- Activities of the Barn Countryside Centre cannot fail to be affected by the presence of the turbine through visual amenity, noise pollution, strobing, impact on trees and wildlife in the area, wildlife corridor.

The 'saynotowindinthenwillows' group made direct contact with GMEU challenging the claims made in the Consultee Comments.

GMEU responded back to the Group, and found no reason to amend their views on the application.

Support -

5 petitions of support signed by 7,7,34,6,14 signatures:

- Environmentally beneficial;
- Would secure the future of the golf club and avoid future alternative proposals such as building sites, tipping grounds, travellers etc;
- Deliver benefits through the Community Trust.

16 letters of support:

- Impact on the surrounding area will be minimal during both construction and operation;
- Renewable energy should be welcomed due to the lack of pollution which would result from such generation via fossil fuel or worse, nuclear fuel;
- Hoped approval given;
- Any initiative which would assist in assuring the continual survival of the Club must be encouraged;
- Sustainable future for the Golf Club;
- Disable golf initiative run by the club needs to be sustained;
- Would prevent the land potentially developed as a building site, industrial estate or landfill.

The applicant has stated that whilst they are aware of the number of objections received, the reporting of the number of responses in support of the application is inaccurate, and not representative of the local community support for the wind turbine.

This is not the case. The system reflects how objections/support is logged, i.e. grouped (i.e. petition) or individually received. The top of the publicity section sets out the exact number of responses received either way.

Those who have expressed an interest have been informed of the Planning Control Committee meeting.

### **Consultations**

**Traffic Section** - No objection subject to conditions.

**Drainage Section** - No objection.

**Environmental Health Contaminated Land** - No comments to make.

**Environmental Health Pollution Control** - The agent has been requested to provide additional information in support of the submitted noise assessment. Indications are that it is unlikely to be of a significant concern given the location, distances from noise sensitive land uses set against ambient background noise levels. However, a further update shall be provided within the Supplementary Agenda.

**Public Rights of Way Officer** - No objection subject to condition regarding the need for footpath diversions and closure orders.

**The Ramblers Association (Bury)** - No response received.

**NATS Ltd** - No objection.

**Manchester Airport - Planning & Infrastructure Department** - No objection.

**Ministry of Defence Safeguarding (wind turbines)** - No objection.

**OFCOM** - Comment for applicant to contact relevant service providers.

**G M Archaeological Advisory Service** - No objection

**Greater Manchester Ecology Unit** - No objection subject to conditions to restrict the timing of vegetation clearance, prevent the spread of invasive species and control the wind speed of the turbine operation.

**JRC (Joint Radio Company)** - No objection.

**Highway England** - No objection.

### **Unitary Development Plan and Policies**

NPPF	National Planning Policy Framework
NPPG	National Planning Policy Guide
MW1	Protection of Mineral Resources
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN2/3	Listed Buildings
EN4	Energy Conservation
EN4/1	Renewable Energy
EN4/2	Energy Efficiency
EN2/4	Historic Parks
EN6	Conservation of the Natural Environment
EN7	Pollution Control
EN7/2	Noise Pollution
OL1	Green Belt
OL1/5	Mineral Extraction and Other Development in the Green Belt
CF1/1	Location of New Community Facilities
EN3	Archaeology

### **Issues and Analysis**

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

### **Assessment:**

**Green Belt Principle** - Para. 14 of the NPPF includes a presumption in favour of sustainable development and for the purposes of decision taking, this means granting permission where development accords with the development plan, and where the development plan is absent/silent or relevant policies are out-of-date unless, specific policies in the NPPF indicate that development should be restricted, one example of which is Green Belt. The implications of paragraph 14 are discussed later in the report.

The proposal represents inappropriate development in the Green Belt as it does not meet the exceptions listed in Paragraph 89 or 90 of the NPPF. Paragraph 87 states that inappropriate development is by definition harmful to the Green Belt and should not be approved except in Very Special Circumstances (VSC).

Additional harm - It is also judged that additional harm is caused to the openness of the Green Belt by the height of the 45m structure, leading to visual impacts when viewed from prominent locations such as the M62 motorway corridor and surrounding areas.

Material considerations in favour - Evidence in the 'Landscape Capacity Study for Wind Energy Developments in the South Pennines' identifies landscapes that could be sensitive to wind turbines and assesses the capacity of the area to accommodate wind energy developments and recognises that the site is low-lying and fragmented by transport

infrastructure. The site is within the Landscape Character Type L: 'Lowland Farmland' which is recorded as having an overall sensitivity of moderate to low. Furthermore, the assessment against sensitivity criteria notes semi-natural woodlands along motorway corridors provide enclosure, that human influences dominate and that the landscape quality is only considered to be fair. In addition, the study finds the Bury and Rochdale capacity area to have opportunities for a medium or even large wind farm within the 'lowland farmland close to the M62 motorway corridor.

It is viewed that the heavy screening provided by the trees in the Prestwich Forest Park area help to minimise views of the turbine from the motorway to a large degree, and furthermore, passing traffic is not likely to be harmed through any introduction of a turbine given the amount of highway distractions that pre exist. In addition, any sightings of the turbine are likely to be viewed against a backdrop of other existing urbanising features in the area in this part of the Green Belt adjacent the two large built-up areas of Whitefield and Prestwich such as lampposts, stanchions, pylons and the floodlights and telecoms mast at Sedgley Park Rugby Club. Cumulative visual impacts are not considered to be an issue in this location, as opposed to other locations where many wind turbines can be visible from the same point, such as in the north of the Borough or even at J18 of the M62 at Simister Island just less than two miles away which offers long distance views of Scout Moor.

Very Special Circumstances - Paragraph 88 gives substantial weight to any harm to the Green Belt and notes that Very Special Circumstances will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm, is clearly outweighed by other considerations. Paragraph 91 states that such VSC may include the wider environmental benefits associated with increased production of energy from renewable sources.

The applicant has put forward the following case for VSC:

- The finance generated will secure the golf club facility, enabling the retention of an appropriate use in the Green Belt and will also contribute towards developing the consented driving range which will benefit the local community and the sport of golf.
- The setup of a community fund to enable access for disadvantaged and disabled adults and children, providing coaching and educational facilities, thereby planning positively for the beneficial use of the Green Belt in accordance with Para 81.
- Retains/protects the site as Open Land.
- Wider environmental benefits associated with increased production of energy from renewable sources, energy produced to be sold to the National Grid, to power 134 houses per annum.
- Unique site specifics including an available grid connection, excellent wind resource, enclosed on 4 sides by built form, well screened, environmental impacts within acceptable standards.

The reasons put forward are not very special on their own, but when taken together the following are capable of forming a case for VSC: the securing of a viable future for the golf club, the commitment to contribute to further coaching and educational facilities for hard to reach groups, the wider environmental benefits from increased production of energy from renewable sources and the enclosed/screened nature of the site.

Summary – Green Belt - The proposal for a wind turbine is considered as inappropriate development in the Green Belt as it does not meet the exceptions listed in Paragraph 89 or 90 of the NPPF. However, it is considered that Very Special Circumstances exist which clearly outweigh the in-principle harm by reason of inappropriateness and any other harm.

**Renewable Energy Principle** - Chapter 10 - Meeting the challenge of climate change, flooding and coastal change of the NPPF recognises the importance of delivering renewable energy, low carbon energy and associated infrastructure. This is central to the economic, social and environmental dimensions of sustainable development. It should be recognised that there is a responsibility all communities to contribute to energy generation from renewable or low carbon sources. Local planning authorities should not require applicants

to demonstrate the overall need for renewable or low carbon energy and recognise that even small scale developments can provide a valuable contribution. Applications should be approved where it can be demonstrated that impacts are or can be made acceptable.

The applicant states that the turbine would generate the equivalent of energy to power 134 houses per annum. It would provide a low carbon and sustainable form of energy supply and contribute to national energy production and reduction in carbon footprint.

Notwithstanding paragraph 91 of the NPPF, which considers impact on the Green Belt, discussed above, the proposal would be compliant with policy guidance in support of the delivery of renewable energy.

### **Technical Analysis/Other material considerations**

**Impact on the visual amenity of the surrounding areas** - It is an accepted principle in planning that there is no 'right to a view'. In this case, the wind turbine would be visible from a number of properties and areas on the periphery of the golf club site, to a greater or lesser extent depending on distance, relationship and position of pre existing landscape features and orientation; as a moving object, the eye would be drawn to the rotation of the blades. Whether the turbine would be unacceptable intrusive or overbearing is a matter of fact and degree.

Those residential properties more likely to have their outlook affected by the siting of the turbine would be those on Park Lane to the north and west and Ross Avenue and Westlands to the east.

A Landscape and Visual Impact Assessment (LVIA) has been submitted with the application which provides an assessment of the relationship of the turbine to the surrounding residential properties, and includes photomontages of viewpoints from outside the site area. A LVIA is a tool by which the significance of and the effects of change resulting from a development would have on the landscape and on people's views and visual amenity.

The LVIA assesses the visual impact of the proposed turbine in terms of its significance of effect and uses the following definitions:

- Negligible - Appropriate in its context and may be difficult to differentiate from the surrounding landscape character;
- Minor - Would cause a barely perceptible effect within the receiving landscape's characteristics;
- Moderate - Would cause a noticeable difference to the landscape;
- Major - Would completely change the character of the landscape for a long period of time or permanently.

The properties/areas most likely to be affected would be:

Westlands - Some of the nearest residential receptors would be the properties on Westlands. Whilst in an elevated position, they would be reasonably well screened by tree planting. There would only be one property who would have a directly facing view of the site, approximately 284m away. The LVIA states that impact would be considered moderate/major. The LPA considers that impact would be moderate given the intervening screening and distance away.

Ross Avenue - The northern part of Ross Avenue is lined by a high hedgerow and substantial tree planting along the boundary with the golf course, and views of the turbine from these properties would be well screened. Given the distance away and the intervening vegetation, the LPA considers impact would be negligible/minor.

To the south of Ross Avenue, the vegetation reduces in height, and there would be a short row of properties which would potentially have first floor views of the turbine. Given there would be a separation distance of more than 400m, impact on these properties would again



be considered by the LPA as minor.

From the public footpath which runs from outside the site on Ross Avenue to the golf course, the turbine would be more visible. There are intervening tree belts and vegetation, and because of this, it would only be the upper part of the turbine which would be visible. Given the turbine would be more than 380m away from this footpath, the LPA considers that impact would be moderate.

Park Lane area - To the more southerly part of Park Lane, the turbine would be most visible through the gap in the hedgerow on the opposite side of the road. The nearest properties would be located approximately 284m away and views are likely to be more limited as these dwellings are bungalows and would only have a ground floor view of the site. The turbine would also be seen within the context of the rugby club, with intervening features of floodlights, signage and natural vegetation. The LVIA considers that visual impact would be moderate from this vantage point, to which the LPA would agree.

The rear of houses on Park Lane to the north of the site would have views across the golf course and of the turbine. There are natural intervening tree belts which feature between the turbine and these properties and these properties would be approximately 600m away. The LVIA considers that there would be no views and therefore no effects from the siting of the turbine in this location. The LPA agrees impact would be negligible.

Properties on Park Avenue would have a view across the golf course and would be approximately 330m away from the turbine. There are intervening tree belts which would partially screen the turbine. The LVIA considers that impact would be moderate/major, to which the LPA would concur.

Public areas - The turbine would be most visible from within the grounds of the golf club and particularly from Philips Park Road public footpath directly to the south, as demonstrated in the view point 8 of the photomontage. It would also be prevalent from the footbridge over the M60. The LVIA considers that impact would be moderate/major, to which the LPA agrees.

From public vistas and footpaths, it is acknowledged the turbine would be a prominent and visible structure. However, these public views would be relatively short term, given the paths are used as throughroutes and as such the sighting of the turbine would be of a temporary nature.

Motorists would have views of the turbine when travelling along the M60 motorway, more so from the east to west direction and impact could be considered moderate/major. However, in terms of a road user receptor, the LVIA considers that there would actually be low sensitivity to visual impact of the turbine, given the speed of travel, views would be very fleeting with travellers experiencing glimpses in passing. The embankment would also offer some screening and as such it is considered the effect to be minor.

From west to east, given the orientation of the turbine in relation to road users, and together with the screening and the lower position of the motorway, the impact would be less so, and therefore have negligible impact.

The LPA would be of the same opinion and conclude impact to be minor/negligible, given the speed of traffic and temporary view of the turbine.

Summary of visual amenity - In terms of impact on the outlook of local residents, it is acknowledged that the turbine would be highly visible from Philips Park Road and the footbridge over the motorway. However, views would only be temporary to users of these areas, and more so from the motorway. Existing tree belts and vegetation would also mitigate some of the visual impact of the turbine. As such, the LPA concludes that on balance, impact would be moderate from these public areas.

In terms of impact on residential amenity, it is considered that the scale of the development, topography of the land, separation distances involved and orientation of properties is such that there would not be a significantly adverse impact on outlook for occupiers, and that from no dwelling would the turbine be visually overbearing, overwhelming or oppressive such that they would be rendered unattractive places in which to live.

Turbines have a temporary life expectancy, and in this case, it would be anticipated that the turbine would operate up to 25 years. It could be conditioned that the turbine be removed from site after this period and the ground restored to its former. The visual impact would therefore not be long term.

Photomontages and methodology adopted - A photomontage is the superimposition of an image onto a photograph for the purpose of creating a realistic representation of proposed or potential changes to a view. Photomontages are a useful aide memoir and a useful tool for the assessment of wind turbine applications.

It is important that photomontages are carried out by a graphics specialist and that best practice is adopted and followed.

The applicant has submitted a methodology which details how this process was undertaken, and the LPA are satisfied it has been carried out to standard.

As such, and in due consideration of all the factors discussed in the section above, it is considered that the siting and location of the turbine would not significantly harm the amenity of residential occupiers who live near to the site, nor users of the public areas of the surrounding areas to warrant a reason for refusal.

**Noise** - 'The assessment and rating of noise from wind farms' (ETSU-R-97) should be used when assessing and rating noise from wind energy developments. The Department of Energy and Climate Change endorses this practice guidance, and this is the guidance to which the application has considered the issue of noise.

The applicant has been requested to provide some additional information in support of the submitted noise assessment. Given the location of the turbine and the distances from the noise sensitive receptors, it is unlikely that there would be significant concern. However, an update to this will be provided in the Supplementary Report.

**Shadow flicker** - Under certain combinations of geographical position and time of day, the sun may pass behind the rotors of a wind turbine and cast a shadow over neighbouring properties. When the blades rotate, the shadow flicks on and off - the impact is known as 'shadow flicker'. Only properties within 130 degrees either side of north relative to the turbines can be affected at these latitudes in the UK - turbines do not cast long shadows on their southern side.

On the 16th March 2011, an independent research study into the phenomenon of shadow flicker from wind turbines was published by the Department of Energy and Climate Change (DECC). The study concluded that there have not been extensive issues with shadow flicker in the UK, and that the frequency of flickering is such that it should not cause significant health risk. In the few cases where problems have arisen, they have been resolved effectively using mitigation measures, in particular turbine shut down systems.

As referenced in renewable UK Appendices - Planning Guidance: Small Wind, November 2011, the yardstick used to assess shadow flicker relates to properties which are located within 10 rotor diameters from a turbine.

The proposed turbine would have 30m diameter blades and so in line with general thinking, shadow flicker needs to be considered in terms of its impact on residential properties within the range of 300m of the turbine. A plan has been submitted which shows the nearest property would be on Westlands, 284m away.

The applicant has submitted a shadow flicker assessment which shows that the nearest receptors would be those properties to the east and west of the turbine location, the most affected being susceptible for 11 hours out of the year. Most receptors would experience less than 10 hours a year. To also account for, is that on many days, the intensity of the sunlight is diminished due to cloud cover or the time of the year, and for example in Winter months in the UK, the sun is lower in the sky and casts longer shadows.

Due to the combination of sunlight and bearings, the reality is that shadow flicker is unlikely to occur for 11 hours a year. Shadow flicker is a phenomenon that can be monitored and turbines shut down when the sun shines at a particular time of day. The predictability and infrequency of shadow flicker makes it a manageable problem and could be readily controlled by mitigation measures and condition.

**Access** - The turbine would be delivered by HGV's via Philips Park Road. As the site would be located approximately 50 to 60m of the road, a temporary construction access road would be required. It would be 3.5m wide and constructed of MOT hardcore, the type which is commonly used as a sub-base for highways and car parks and would be overlain onto a protective mat.

The highways section have raised no objection to either the siting or the proposed access to the site, with a condition recommended that a Construction Management Plan (CMP) be submitted to show details and a schedule of works.

In terms of visual impact, the road would be of a temporary nature to accommodate the transportation of the turbine and the equipment needed for its installation. It is also proposed to lay a surfacing to protect the ground from any potential pollution. The applicant would be agreeable to a condition which would require the submission of a CMP to include details of returning the land to its former state, and as such it is considered there would not be a detrimental impact on the Green Belt.

**Footpaths** - The proposed location of the turbine places it close to a network of public rights of way at the southern edge of the golf course. The general consensus is that a turbine should be sited a distance of its total height +10% metres from the nearest public right of way. Given that the turbine would be 45m in height, the turbine would need to be 49.50m away. This would require some rationalisation of the public right of way network at this location before this development could go ahead and would require diversions and closures.

The applicant has submitted a proposed plan to show how a distance of 50m would be achieved, and to which has been agreed in principle by the Footpath's Officer.

The proposals were also discussed at the Local Access Forum and no objections were raised in principle to the proposals.

The details and implementation in relation to the footpath diversions and closures could be covered by a suitably worded condition, and also under the Highways Act 1980.

**Heritage** - Paragraph 129 of Chapter 12 - Conserving and enhancing the historic environment of the NPPF seeks to ensure that consideration is given to the impact of a proposal on a heritage asset, to avoid or minimise conflict between the heritage asset's conservation and any aspect of the proposal. Paragraph 132 considers the impact of a proposed development on the significance of a designated asset, and the weight which should be given to the asset's conservation. The more important the asset, the greater the weight should be. Paragraph 134 states that where a development proposal would lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.

Philips Park itself is not a listed park or garden, but contains The Gate Lodge, the

Conservatory and the Garden Temple, all of which are listed. The nearest would be the Gate Lodge and Gate Piers and walls which are also listed, which would be more than 250m away from the proposed turbine. The view points submitted with the LVIA demonstrate that the most likely impact would be from the vicinity of the Grade II Gate Lodge.

There are intervening features in the form of tree screening, flood lights and communications mast and the topography of the land, the siting of the turbine and the distance away is such that the turbine would not be readily viewed against the setting of any of the heritage assets.

As such, it is considered that the overall harm would be less than substantial on the heritage assets in the vicinity, and there would not be an adverse impact on the preservation of the heritage assets, and the development would accord with chapter 12 of the NPPF.

**Ecology** - An Ecological survey and assessment has been carried out and concludes that the proposed location of the wind turbine is not within or close to any sites designated for their nature conservation value. The footprint of the turbine base would be relatively small and construction would not affect any habitats of substantive importance. Although birds would be present, the surrounding habitat is not suitable for supporting bird species known to be at particular risk of collision with turbine blades (eg geese, swans, larger raptors and waders). The surrounding habitat is suitable for use by foraging bats.

GMEU have been consulted and accept the report has been carried out to appropriate standards, and have raised no objection to the proposal on nature conservation grounds and recommend conditions and informatives regarding treatment of Japanese knotweed and Himalayan balsam, restriction of vegetation clearance to avoid bird nesting disturbance and reasonable avoidance methods used to avoid harm to reptiles or amphibians. The ecology report also recommends that wind speeds are controlled to protect foraging bats.

As such, the proposal is considered to be acceptable and comply with chapter 11 - conserving and enhancing the natural environment of the NPPF.

**Highways England** - It has been confirmed that the turbine would be situated an acceptable offset distance from the M60 trunk road highway boundary.

**Archaeology** - The proposed development would not threaten the known or suspected archaeological heritage and GMAU have no reason to impose any requirements in this respect.

**Safety** -

- Buildings - Fall over distance of a turbine + 10% is often used as a safe separation distance. There are no buildings in the vicinity which would be affected by the location of the proposed turbine.
- Air traffic and safety - There have been no objections by the Planning and Infrastructure Department of Manchester Airport.
- NATS - No objections have been raised from the safeguarding department.
- Strategic Road Network - Highways England are satisfied the turbine would be located an acceptable offset distance from the highway boundary.

**Pre-application Community Consultation** - Whitefield Golf Club (WGC) state that extensive consultations were carried out with the local community pre-submission of the application.

Two public consultations and presentations were carried out on 2/3/15 and 11/5/15. WGC members undertook one leaflet drop w/c 16/2/15 and 2 further leaflets drops on 22/4/15 and 6/5/2015 prior to these meetings, the second leaflet drop arising from the concerns that not all attendees had received the consultation flyer. The number of properties who were notified of the proposed development and meetings has not been specified by the applicant,

although a plan has been submitted to show the areas to the west and east of the proposed site.

Criticism was made by residents to the poor quality of the presentation literature to which the golf club responded with a more detailed flyer. Information was posted on their website.

The first consultation event was attended by approx 40 members of the public and 3 Local Councillors and the second by 40 to 50 people and 2 Councillors.

The presentations raised queries and a number of concerns over the potential ecological, highways, radio signals, noise, access, money generated, local property values, visual and shadow flicker impacts.

Updates to reports were made in response to specific questions raised, and the applicant submits that 6 clarifications were made as a result of the consultations, relating to -

1. inclusion of technical considerations;
2. testing of 3 receptors to evidence worst case noise implications;
3. inclusion of a road specification and agreement to a Construction Management Plan;
4. agreement to remove Japanese knotweed and Himalayan balsam;
5. provide details of the Whitefield Community Fund;
6. agreement by WGC to remove the telecommunications mast.

The publication "Community Benefits from Onshore Wind Developments Best Practice Guidance for England (October 2014) identifies that there are opportunities to secure benefits for the community, and these should grow out of discussions between the community and the developer, and should be relative in scale and nature to the proposed development.

In terms of community benefit, WGC would commit to a "Whitefield Community Fund", to part fund the provision of a new golf driving range to enable access to golf for disadvantaged and disabled adults and children. It would also provide 10% of the net profit generated by the turbine to finance other educational and/or sporting facilities in the community. The proposal to engage in such discussions should be endorsed and encouraged.

Following the leaflet drops and presentations, the full scope of the planning application documents and plans were published on the golf club website for a 2 week period before the submission of the application.

The applicant concluded that the planning application documentation and supporting information evidences that the single wind turbine would be located in a suitable location, would be technically deliverable, has planning policy support and would assist in providing a new Golf Driving range, as well as funding other community programmes that apply to the Whitefield Community Fund.

### **Land Use Principle - Planning Practice Guidance and Ministerial Statement**

Section 38 (6) of the Town and Country Planning and Compensation Act states that decisions must be taken in accordance with the development plan unless there are material considerations that indicate otherwise.

A material consideration and overriding factor, which would outweigh any other planning consideration is the Written Ministerial Statement of 18th June 2015.

Paragraph 032 Ref ID:5-032-150618 and 033 Ref ID:5-033-150618 of the Renewable and Low Carbon Energy section of the Planning Practice Guidance (PPG) includes new guidance relating to applications for wind energy development, and this refers to a Written Ministerial Statement (WMS) made by the Secretary of State for Communities and Local Government on 18 June 2015. The PPG and the Ministerial Statement are therefore a material consideration carrying substantial weight.



Paragraph 033 (ibid) of the PPG sets out that local planning authorities should only grant planning permission for applications for wind energy development involving one or more wind turbines if:

- the development site is in an area identified as suitable for wind energy development in a Local or Neighbourhood Plan; and
- following consultation, it can be demonstrated that the planning impacts identified by affected local communities have been fully addressed and therefore the proposal has their backing.

Paragraph 032 (ibid) clarifies that suitable areas for wind energy will need to have been allocated clearly in a Local or Neighbourhood Plan.

As referred to in the PPG, the Written Ministerial Statement includes transitional arrangements for applications involving wind energy development which have already been submitted prior to the WMS but where the development plan does not identify suitable sites. In such instances, local planning authorities can find a proposal acceptable if, following consultation, they are satisfied it has addressed the planning impacts identified by affected local communities and therefore has their backing.

The transitional arrangements do not apply to this application as it was submitted after the issuing of the Ministerial Statement and the revisions to the PPG.

The Bury UDP is the statutory development plan and does not allocate any specific areas as being suitable for wind energy development. The applicant was invited to respond to the PPG and Written Ministerial Statement. They referred to the UDP policies at EN4, OL1/5 and supporting evidence in the 'Landscape Capacity Study for Wind Energy Developments in the South Pennines'. None of the policies quoted provide allocations which comply with the policy guidance and statement, and the latter study has the status of evidence base and is not statutory policy.

In terms of Criteria 2 of the WMS, the applicant considers the technical reports submitted with the planning application fully address the planning impacts identified by the local community members that attended the pre-application consultations and that the proposals do have the backing of the majority of the local community.

It is considered that the proposal fails to comply with the PPG and Ministerial Statement as it fails the first test of being identified as suitable for wind energy development in a Local Plan. Whether the second criterion of community support is addressed or not is immaterial as both criteria of Paragraph 032 (ibid) PPG need to be satisfied.

### **Land Use Principle - Summary**

Paragraph 14 of the NPPF has a presumption in favour of sustainable development, and for decision-taking this means that, unless material considerations indicate otherwise, permission should be granted where development accords with the development plan and in the event of the development plan being absent, silent or relevant policies out-of-date, unless specific policies in the NPPF indicate that development should be restricted.

The Bury UDP was adopted in August 1997 and is therefore relevant to the consideration of Paragraph 14. In respect of the proposal for a wind turbine, the specific policy indicating development should be restricted is Green Belt. Whilst the proposal is inappropriate in the Green Belt it is considered that Very Special Circumstances exist which clearly outweigh the in-principle harm by reason of inappropriateness and any other harm.

However, the proposal does not accord with the advice within the NPPG and Ministerial Statement of 18 June 2015 as the site is not allocated in the development plan as an area suitable for wind energy development. This is a material consideration of substantial weight.

It is the Council's view therefore that the material consideration of the NPPG and Ministerial

Statement indicate that the presumption in favour of sustainable development in Paragraph 14 of the NPPF, insofar as decision-taking provisions are concerned, should not be followed in this case and the application should be refused.

**Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

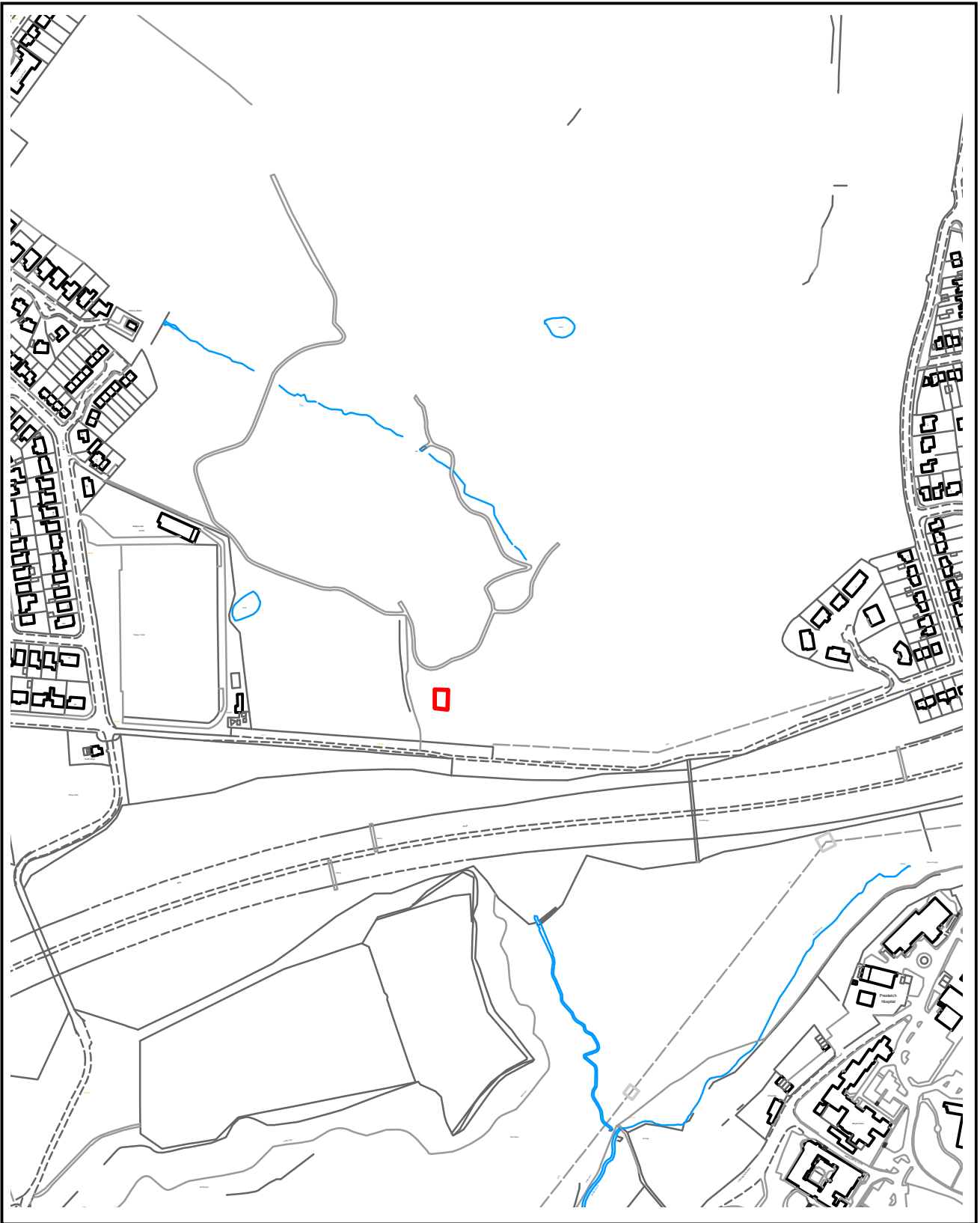
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**Recommendation:** Refuse

**Conditions/ Reasons**

1. The proposed development for a wind turbine is not in an area identified as suitable for wind energy in the Bury UDP. The proposal is therefore contrary to Paragraph 032 Ref ID: 5-150618 and 033 Ref ID: 5-033-150618 of the Planning Practice Guidance and the Written Ministerial Statement made by the Secretary of State for Communities and Local Government on 18 June 2015.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**



**PLANNING APPLICATION LOCATION PLAN**

**APP. NO 58874**

**ADDRESS: Whitefield Golf Club  
Higher Lane  
Whitefield**

**Planning, Environmental and Regulatory Services 1:1250**



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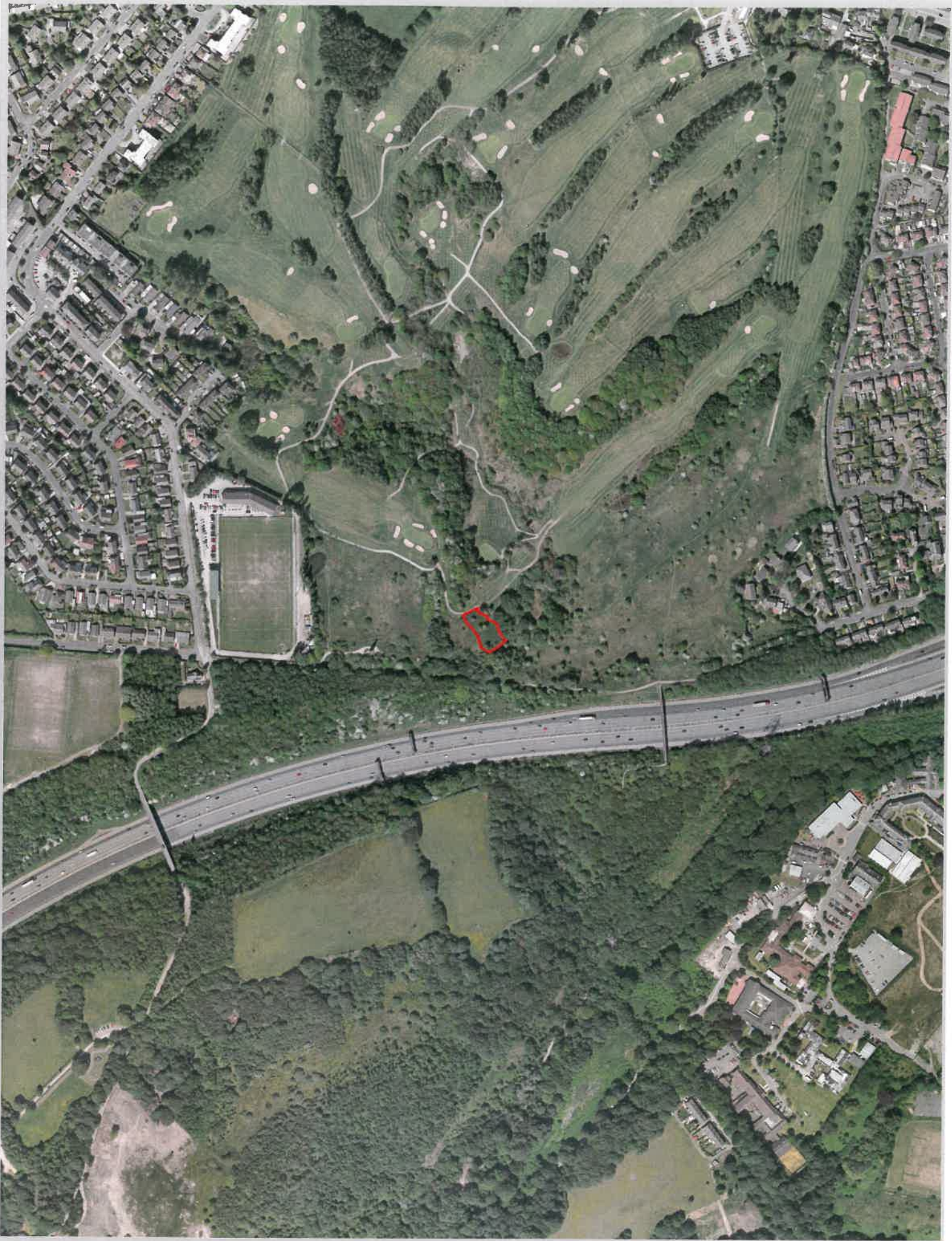


**SCALE:** 1:1250    **DATE:** 21/09/2015

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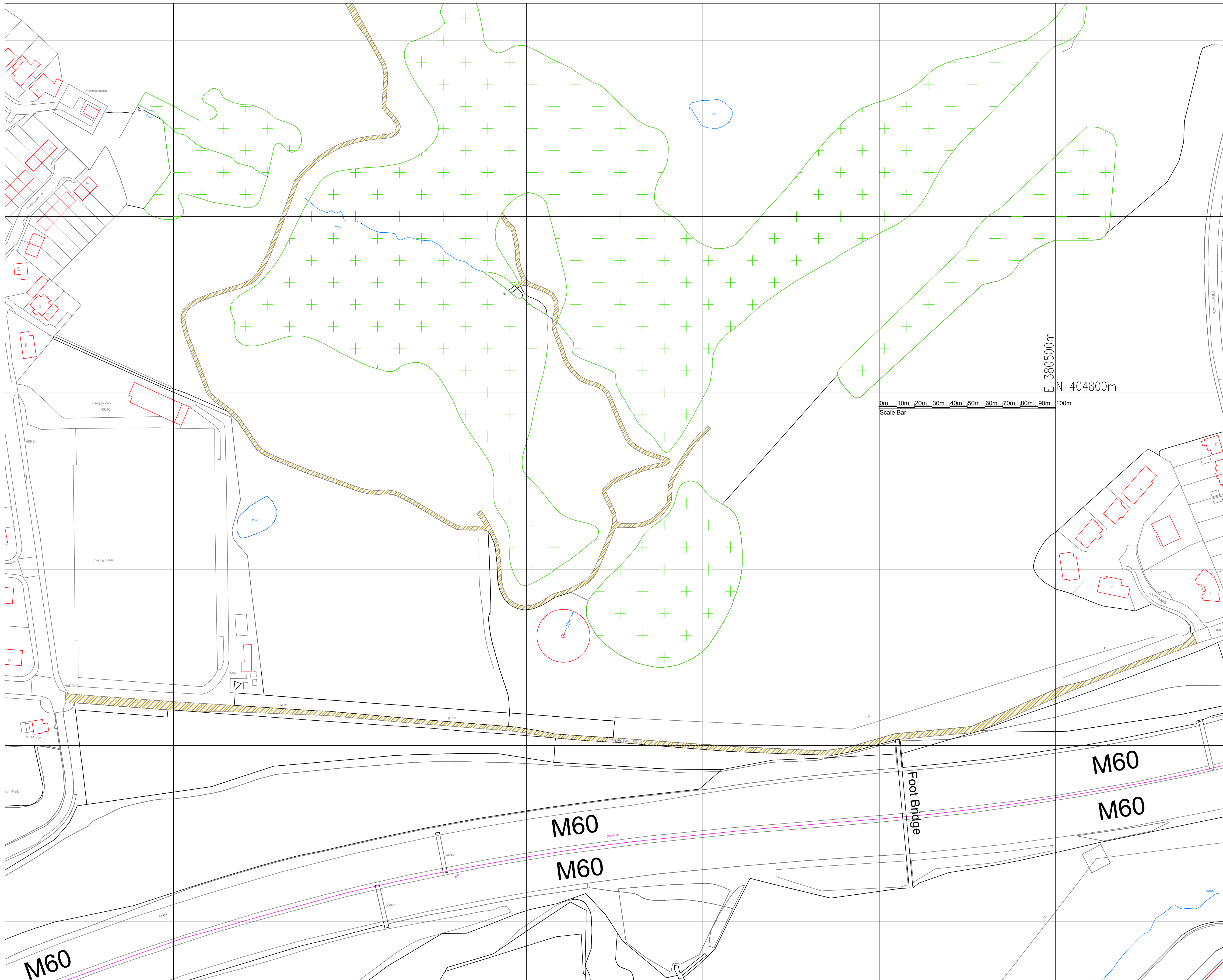
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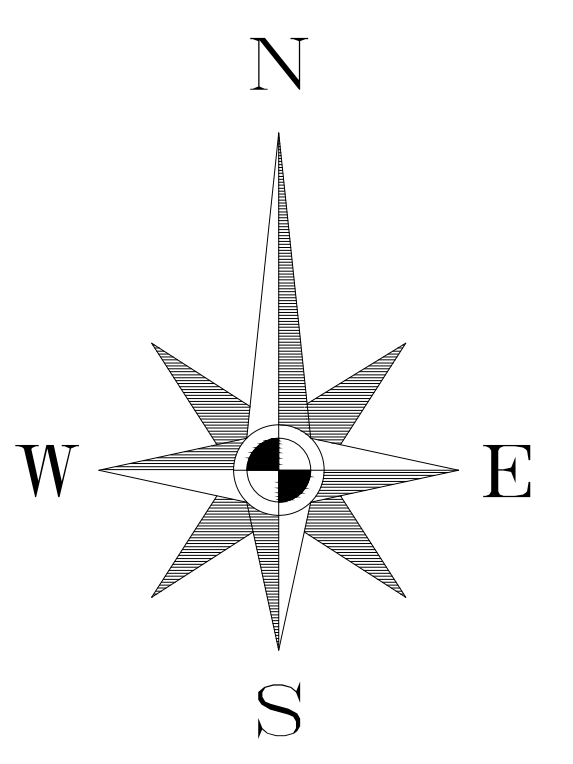


**Bury**  
COUNCIL






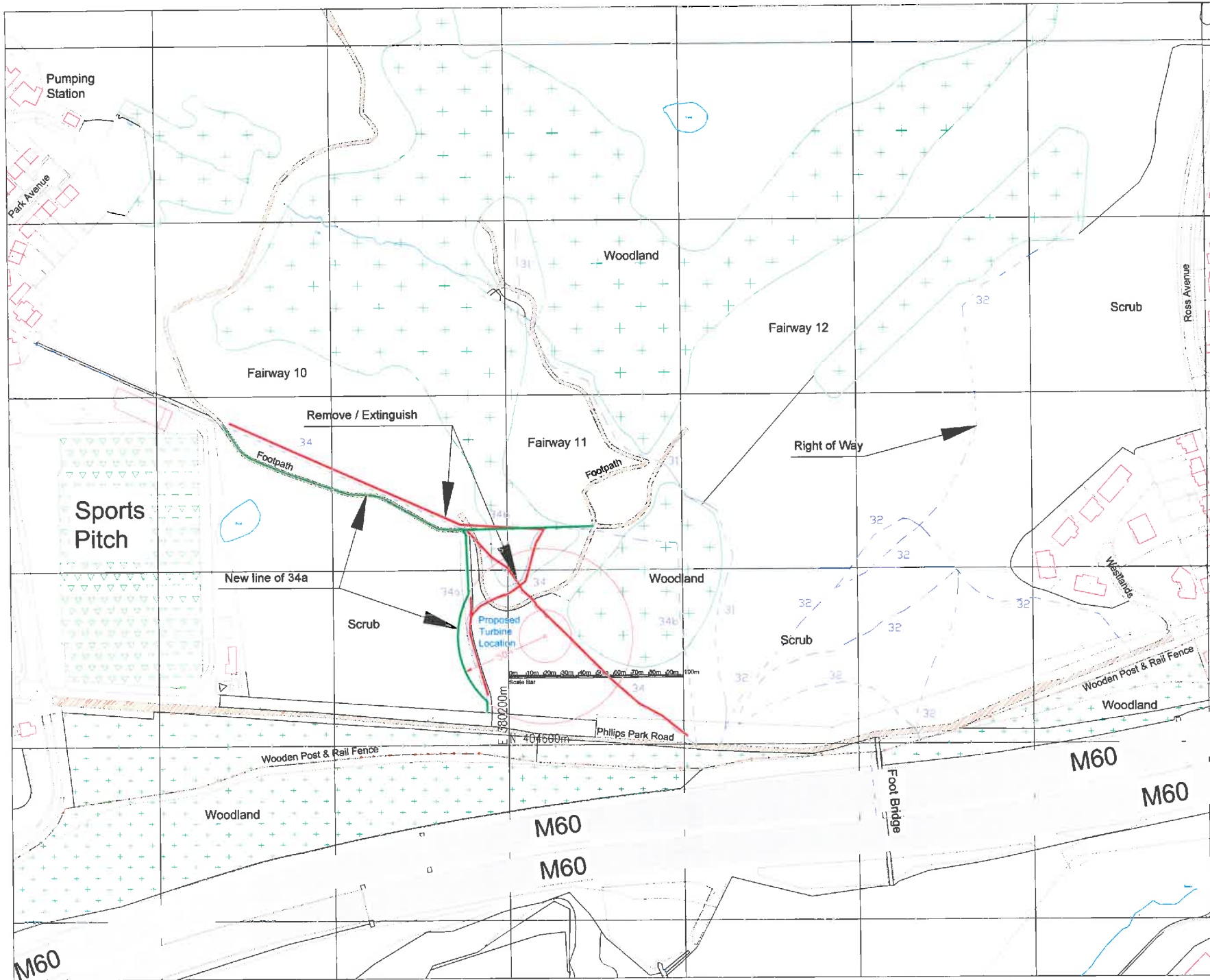
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Map by Ordnance Survey.



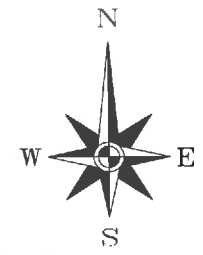
Revision:	Description:	Date:
Drawing Title:	Location Plan	
Project:	Whitefield Golf Club, Higher Lane, Whitefield, Manchester, M45 7EZ	
Client:	David Ballan c/o Russell Adams	Drawing Status: OS Map Data


**Land & Measured Building Surveying**  
 Site Engineering  
 Building Plans  
 www.d2r.eu 3D Scanning  
 Nationwide Tel: 01524 382502 email: info@d2r.eu

Date:	Scale:	Drawn:	Checked:
13th July 2015	1:1000 @ A1	WTB	WTB
Drawing Number:	D2R - WF - 01	Client:	Rev:
			0



Notes:  
 Map by Ordnance Survey. Enhanced by D2R for clarity and turbine position added. Rights of way for guidance only, digitally traced from an image file.



- Map Key:**
- Existing Public Right of Way
  - Existing Golf Club Footpath
  - New Line of Footpaths 34a & 34b
  - Right of Way Extinguished

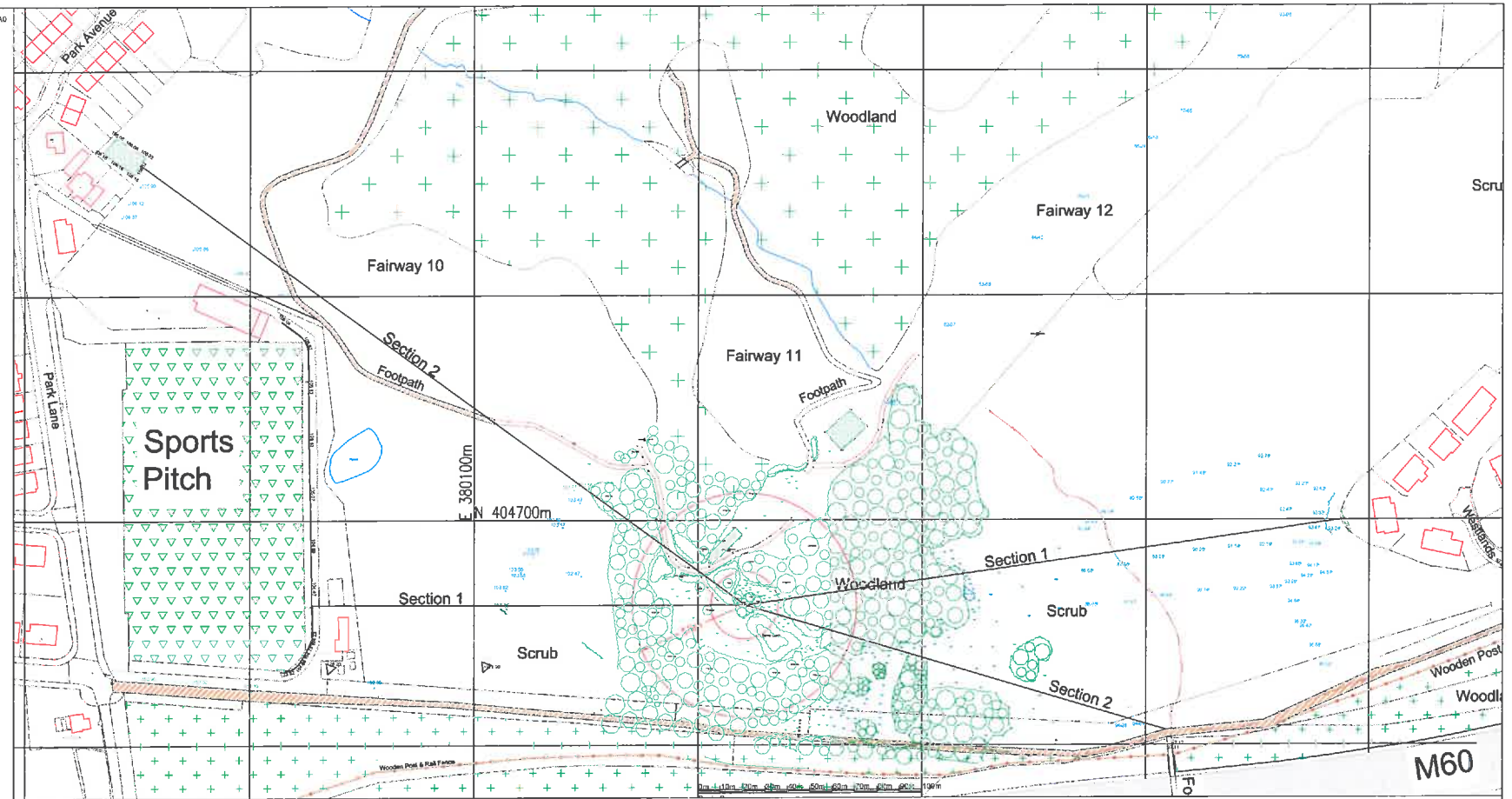
Revision	Description	Date
B	34a Route Amended	29th July 2015
A	Key added	14th July 2015

Drawing Title	Footpath Improvement & Extinguishing Plan		
Project	Whitefield Golf Club, High Lane, Whitefield, Manchester, M41 7EZ		
Client	David Balkin c/o Russell Adams		
Drawing Name	OS Map Data		
Drawn	Land & Measured Building Surveying		
Checked	Site Engineering		
Drawn	Building Plans		
Checked	3D Scanning		
Nationwide Tel: 01524 382502 email: info@d2r.eu			
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D2R - WF - 05			



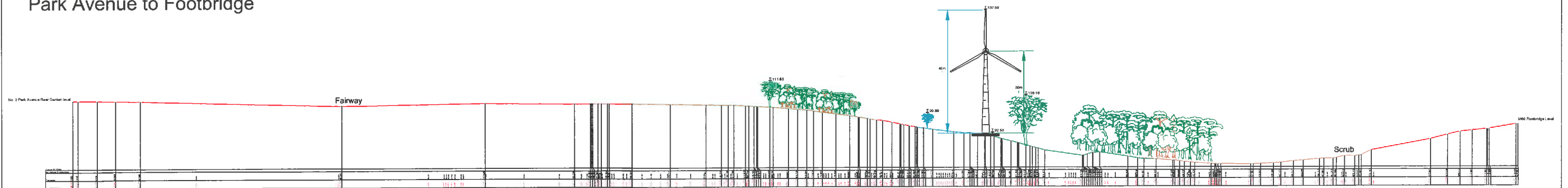
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**Section 1**  
Rugby Club to Westlands

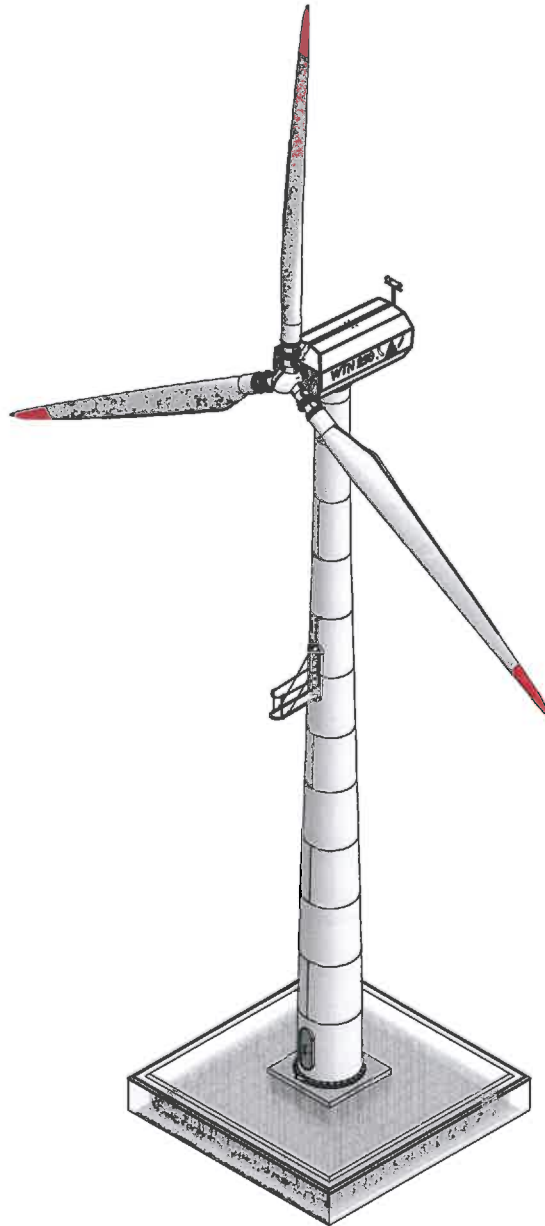



**Section 2**  
Park Avenue to Footbridge

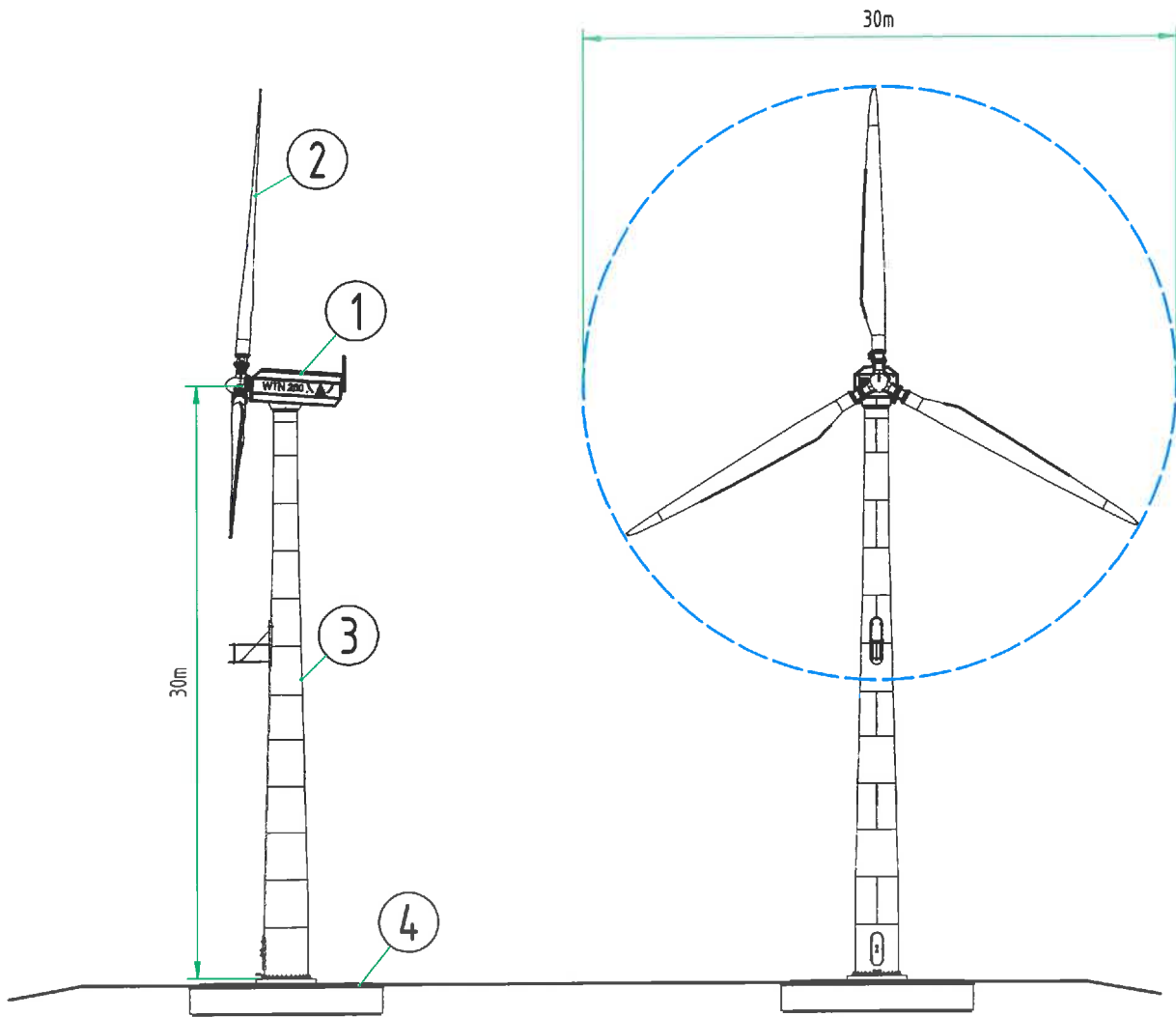



Notes:  
Survey to OS National Grid.

Revision	Description	Date
A	Turbine Height Corrected	18th Sep 2015
Drawing Title: Cross Sections		
Project: Whitfield Golf Club, Higher Lane, Whitfield, Manchester, M46 1EJ		
Client: David Bullen c/o Russell Adams		
Drawing: Survey Information		
Land & Measured Building Surveying Site Engineering Building Plans 3D Scanning Nationwide Tel: 01524 382502 email: info@d2r.eu		
Date: 17th Sep 2015	Scale: 1:500 @ A0	Drawn: WTB
Checked: WTB	Client: WTB	Sheet: A
Drawing No: 02R-WF-02		



2	1	Rotor	250-20-0000				
1	1	Nacelle	250-10-0000				
3	1	Tubular Tower 30m	250-33-0000				
4	1	Foundation Tubular Tower 30m	250-50-0000				
Pos.	No.	Description	Drawing no. / Specification	Material			
			Weight: 374459,91 kg	Material:			
			According to the law we reserve the right of property of the present drawing, which cannot be printed or transmitted without our written authorization.				
Name: WTN 250 Tubular Tower 30m			Drawing No.: 250-00-1500	Scale: 1:175 at A3			
Drawn:	Date: 28.01.2010	Name: Andresen	Production tolerance: DIN ISO 2768-T1 m (mittel)	Correction:	Date:	Name:	Page: 1 of 2
Check:							

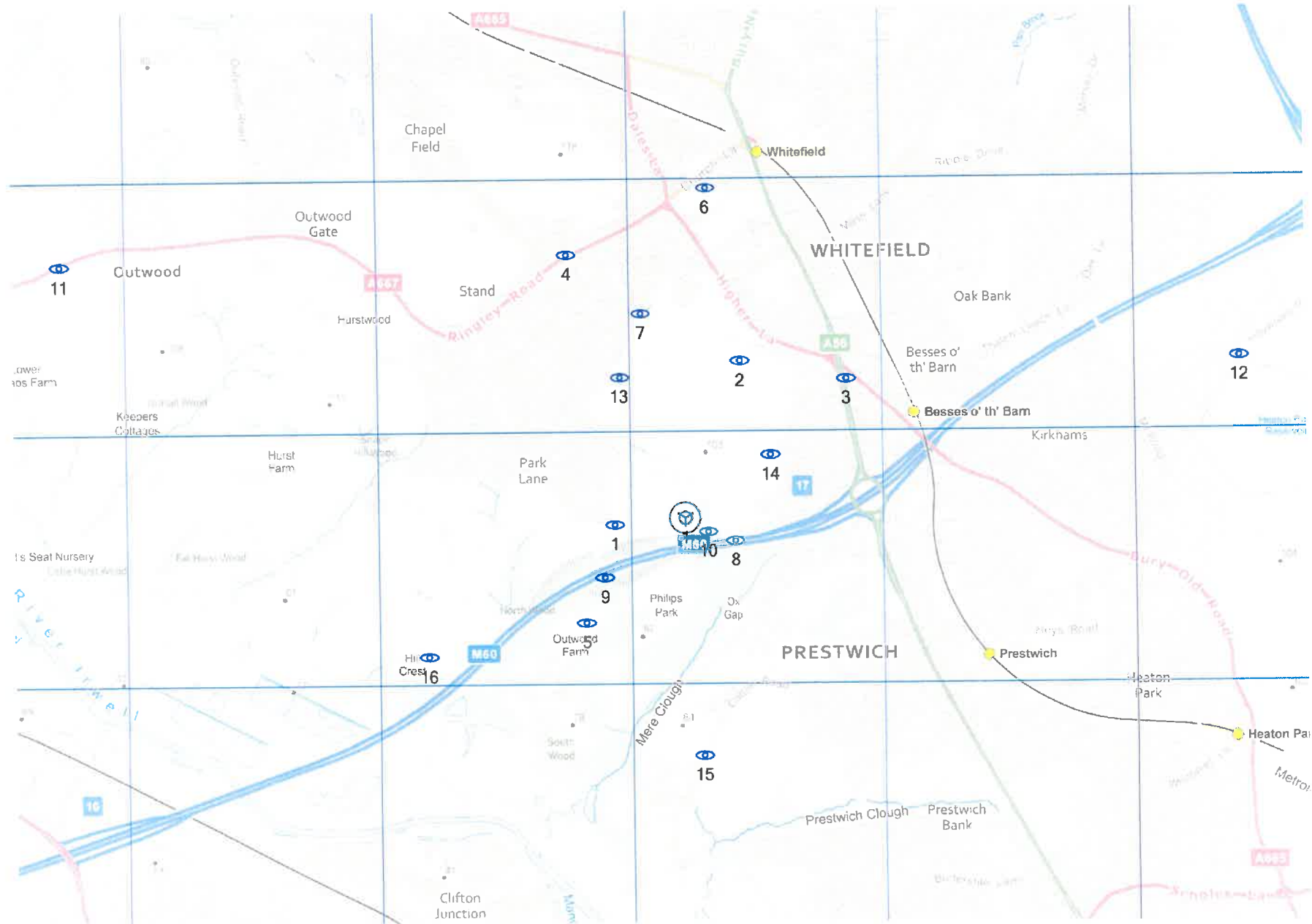


		Weight: 374459,91 kg		Material:	
		According to the law we reserve the right of property of the present drawing, which cannot be printed or transmitted without our written authorization.			
Name: WTN 250 Tubular Tower 30m		Drawing No.: 250-00-1500		Scale: 1:250 at A3	
Date: 28.01.2010	Name: Andresen	Production tolerance: DIN ISO 2768-T1 m (mittel)	Correction:	Date:	Name:
Drawn:	Check:				Page: 2 of 2



# Title of site: Whitefield Golf Club

Site and Viewpoint Location Map





**Viewpoint 1: Phillips Park Road Lodge**

When viewed at a comfortable arm's length (approx. 500mm) this image is representative of the maximum field of view of clear vision

Distance to nearest Turbine: 282m  
Date: 09/02/2015 Time: 13:33

Camera: Canon Eos 5D  
Grid Coords: x 379935, y 404635 Direction: East

Focal Length: 50mm

Camera Height: 1.6m





**Viewpoint 2: Broxups 71/Whitefield Golf Club**

When viewed at a comfortable arm's length (approx. 500mm) this image is representative of the maximum field of view of clear vision

Distance to nearest Turbine: 660m  
Date: 09/02/2015 Time: 12:32

Camera: Canon Eos 5D  
Grid Coords: x 380437, y 405277 Direction: South West

Focal Length: 50mm Camera Height: 1.6m





**Viewpoint 7: Park Lane**

When viewed at a comfortable arm's length (approx. 500mm) this image is representative of the maximum field of view of clear vision

Distance to nearest Turbine: 830m  
Date: 09/02/2015 Time: 13:15

Camera: Canon Eos 5D  
Grid Coords: x 380047, y 405471 Direction: South East

Focal Length: 50mm Camera Height: 1.6m



**Viewpoint 8: Footbridge Over M60**

When viewed at a comfortable arm's length (approx. 500mm) this image is representative of the maximum field of view of clear vision

Distance to nearest Turbine: 217m  
Date: 09/02/2015 Time: 13:52

Camera: Canon Eos 5D  
Grid Coords: x 380413, y 404566 Direction: North West

Focal Length: 50mm Camera Height: 1.6m





**Viewpoint 9: Bridge over M60**

When viewed at a comfortable arm's length (approx. 500mm) this image is representative of the maximum field of view of clear vision

Distance to nearest Turbine: 396m  
Date: 09/02/2015 Time: 14:19

Camera: Canon Eos 5D  
Grid Coords: x 379895, y 404422 Direction: North East

Focal Length: 50mm

Camera Height: 1.6m



**Viewpoint 10: National Cycle Network/Footpath**

When viewed at a comfortable arm's length (approx. 500mm) this image is representative of the maximum field of view of clear vision

Distance to nearest Turbine: 93m    Camera: Canon Eos 5D    Focal Length: 50mm    Camera Height: 1.6m  
Date: 09/02/2015    Time: 13:44    Grid Coords: x 380292, y 404603    Direction: North West





**Viewpoint 13: Footpath from Park Lane**

When viewed at a comfortable arm's length (approx. 500mm) this image is representative of the maximum field of view of clear vision

Distance to nearest Turbine: 614m  
Date: 09/02/2015 Time: 13:23

Camera: Canon Eos 5D  
Grid Coords: x 379955, y 405220 Direction: South East

Focal Length: 50mm

Camera Height: 1.6m





**Viewpoint 14: Footpath from Ross Avenue**

When viewed at a comfortable arm's length (approx. 500mm) this image is representative of the maximum field of view of clear vision

Distance to nearest Turbine: 423m  
Date: 09/02/2015 Time: 14:41

Camera: Canon Eos 5D  
Grid Coords: x 380553, y 404905 Direction: South West

Focal Length: 50mm Camera Height: 1.6m



**Ward:** Whitefield + Unsworth - Besses

Item 05

**Applicant:** Mr Ali Khan

**Location:** 116 Bury New Road, Whitefield, Manchester, M45 6AD

**Proposal:** Retrospective application for change of use of part of ground floor from offices (A2) to taxi booking office (Sui Generis)

**Application Ref:** 59051/Full

**Target Date:** 25/09/2015

**Recommendation:** Approve with Conditions

### **Description**

The site is part of a two storey brick building located at the end of a row of terraces on York Street. The front elevation faces on to Bury New Road and has a glazed shop front and entrance. Immediately outside is a controlled crossing. The adjoining premises is commercial and the site is within Whitefield District Centre. There are residential properties to the rear terraced streets and directly opposite.

The application is retrospective and relates to the ground floor which is being operated as a private hire booking office. The service is for telephone bookings only with no customers or private hire drivers visiting. The business operates 24 hours a day with 2 full time staff and 3 part time.

### **Relevant Planning History**

20178/87 – Change of use from shop/living accommodation to 2 No. shop units with offices over and new shop fronts at 116 & 116A Bury New Road, Whitefield. AC -12/11/87.

48509 - Change of use of ground floor from shop (Use Class A) to Estate Agency (Use Class A2); Modification of external elevation - AC 02/10/07.

59052 - Retrospective application for retention of 1 no. non-illuminated hoarding sign on front elevation - Refused 08/09/15.

**Publicity** - 28 notification letters were sent to addresses at 114, 114A, 116, 118, First Floor 118, 120, 155, 155A, 157, 159, 161, 163, 165, Flats 1&2 165 Bury New Road, Higher Lane County Primary. Wilson & Garden Ltd, York Street, Photo & Optical York Street. 1,1A,2,3,4 York Street, 3-9 Livsey Street.

Two objections have been received from 157 & 159 Bury New Road their concerns in summary are:-

- Since the office opened, on street parking on Egerton Road, which was difficult, has become almost impossible.
- Taxis park on both sides of Egerton Road blocking the path. There are saloon cars and 8 & 14 seat mini buses.
- A residents parking scheme may help.
- There have been drivers outside at all hours and drunken customers waiting outside at weekends.
- This is planning permission via the back door and very underhand by both the business and the Council.
- Vehicles park on the pavement next to the fence to No.157 restricting maintenance to the property.
- A large advertising board has been erected opposite the window to No.157 which is inappropriate and the largest sign on Bury New road. It brings down the value of the property.

- One side of No.157 overlooks a massage parlour with boarded windows. In 30 years nothing positive has been done. Who approved this mess ?

The objectors have been notified of the Planning Control Committee meeting.

## **Consultations**

**Traffic Section** - No comments received within the statutory time period.

## **Unitary Development Plan and Policies**

HT2/8	Taxi and Private Hire Businesses
S1/3	Shopping in District Centres
S2/4	Control of Non-Retail Uses in All Other Areas
EC4/1	Small Businesses
HT2/4	Car Parking and New Development

## **Issues and Analysis**

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

**Policy** - Policy S2/4 states that outside the main shopping area of town and district centres, changes of use from a retail shop to a non-retail use will be permitted, subject to the proposal being appropriate in scale and character to the area and the proposal should not result in an over concentration or grouping of uses. The proposal should retain the display window and allow for access for the mobility impaired. Adequate servicing and parking should be provided.

UDP Policy HT2/8 - Taxi and Private Hire Businesses states that proposals for taxi or private hire businesses will be looked upon favourably providing that adequate car parking is provided for employees and vehicles used in connection with the business, there will not be an unacceptable adverse effect on the amenities of neighbouring residents or occupiers or the local highway network, in terms of road safety and traffic circulation.

The site is not located within either the primary or secondary shopping area of the district centre and would not result in the loss of an A1 use with previous permission for A2 granted. It would not lead to an over-concentration of the use and as a sui generis use any further change would require an application and allow further assessment.

The premises has an existing shop front and display window which would maintain an active frontage. The restriction on the customers visiting the premises would mean that the use would not rely on or create any footfall. However the unit is located towards the end of what is a large district centre and it would provide a service that would be appropriate within a local area. Therefore, it is considered that the proposed development would not harm the vitality and viability of the district centre and would not conflict with the aims of Policies S1/3 and S2/4 of the adopted Unitary Development Plan.

**Parking and Access** - There is no dedicated parking to the premises. The private hire vehicles are owned by the drivers themselves and kept at their own residences at the end of a shift. All fees due to the office by the drivers are collected from their residences. With no visiting customers, only the booking staff would be at the premises. The number employed would be similar to that of a retail use where parking would have to be on street within the area or staff make use of public transport which the area has high level access to.

Also the site has benefit of a lawful A2 use where it would be expected that staff would generate some level of traffic.

The front entrance does not have disabled access and the agent has stated that it would not be practical to modify it. However there are no visiting members of the public proposed for the scheme.

**Visual amenity** - No external alterations are proposed. The frontage has a glazed window with a fascia sign above the shop. The board sign adjacent the entrance was refused under a separate advert application submitted alongside this proposal. As such it is not considered that the proposed development would look out of place within the locality.

**Residential amenity** - The use would operate 24 hours. With the business based on telephone bookings and without customers or hire car drivers visiting the premises, the use is not one that would give rise to any noise or disturbance above the existing activity within the district centre. This system of operation would be reinforced by a condition restricting the use to telephone bookings and restricting customer and driver facilities on the premises. The proposed development would be in accordance with the UDP Policies listed.

**Response to objection** - The issues relating to noise and traffic are addressed in the above report.

On street parking and residents parking schemes are not matters under planning control. The large advertising board has been refused under application 59052.

### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

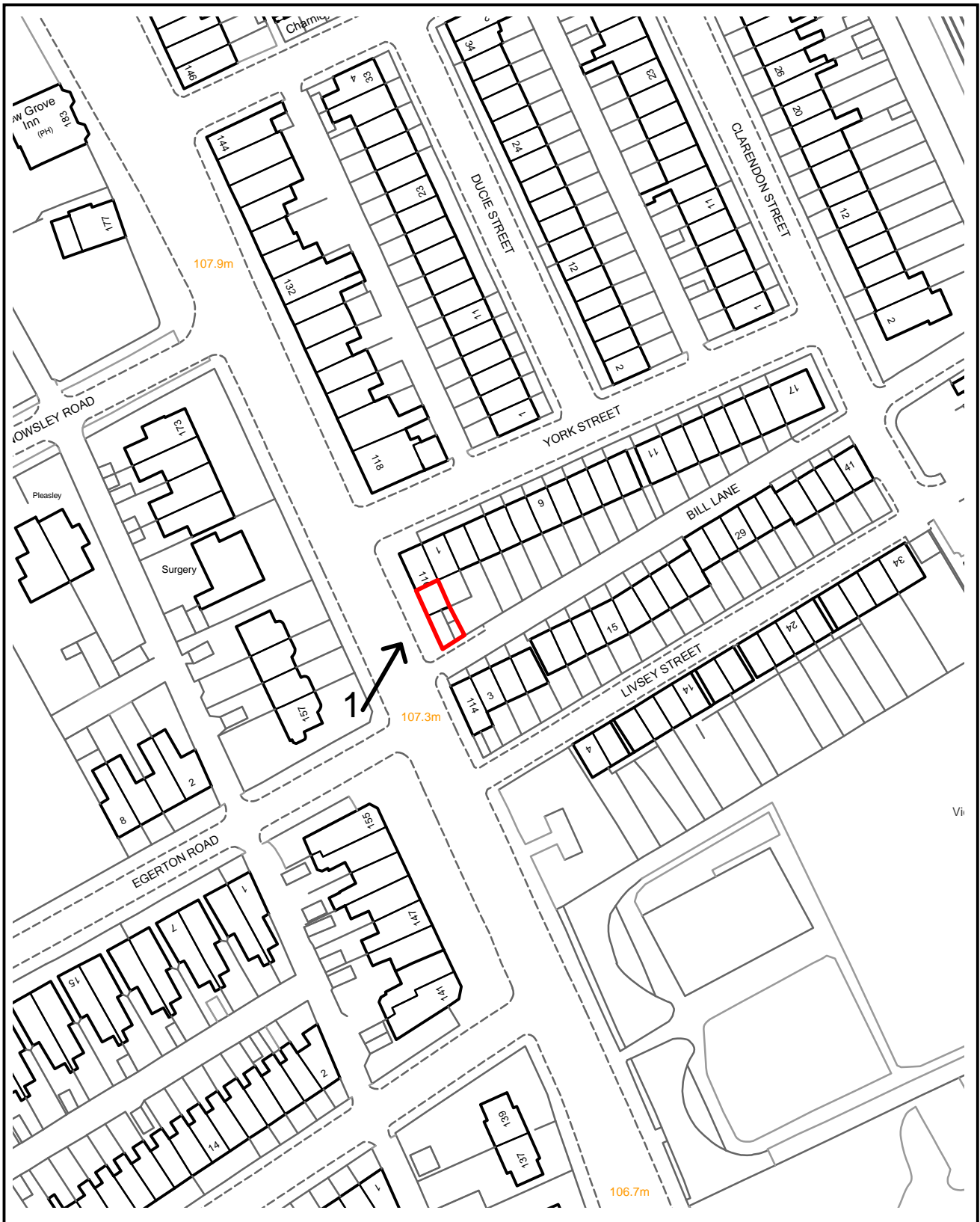
**Recommendation:** Approve with Conditions

#### **Conditions/ Reasons**

1. This decision relates to drawings numbered Sheet 1, Sheet 2 and the development shall not be carried out except in accordance with the drawings hereby approved.  
Reason: For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
2. The use hereby approved shall be limited solely to telephone bookings. There shall be no facilities at the premises for customer pick up, driver waiting or rest and refreshment.  
Reason: The use relies upon on-street parking and therefore in the interests of residential amenity and highway safety pursuant to UDP Policy HT2/8 Taxi and Private Hire Businesses.
3. No private hire vehicles belonging to the applicant or those belonging to freelance drivers operating through the communications system at the premises, shall attend the office hereby permitted, including for the purposes of waiting or taking orders and instruction, collecting clients or for the purpose of taking refreshment.  
Reason: The use relies upon on-street parking and therefore in the interests of residential amenity and highway safety pursuant to UDP Policy HT2/8 Taxi and Private Hire Businesses.

For further information on the application please contact **Jane Langan** on **0161 253 5316**

# Viewpoints



## PLANNING APPLICATION LOCATION PLAN

**APP. NO 59051**

**ADDRESS: 116 Bury New Road  
Whitefield**



**Planning, Environmental and Regulatory Services 1:1250**

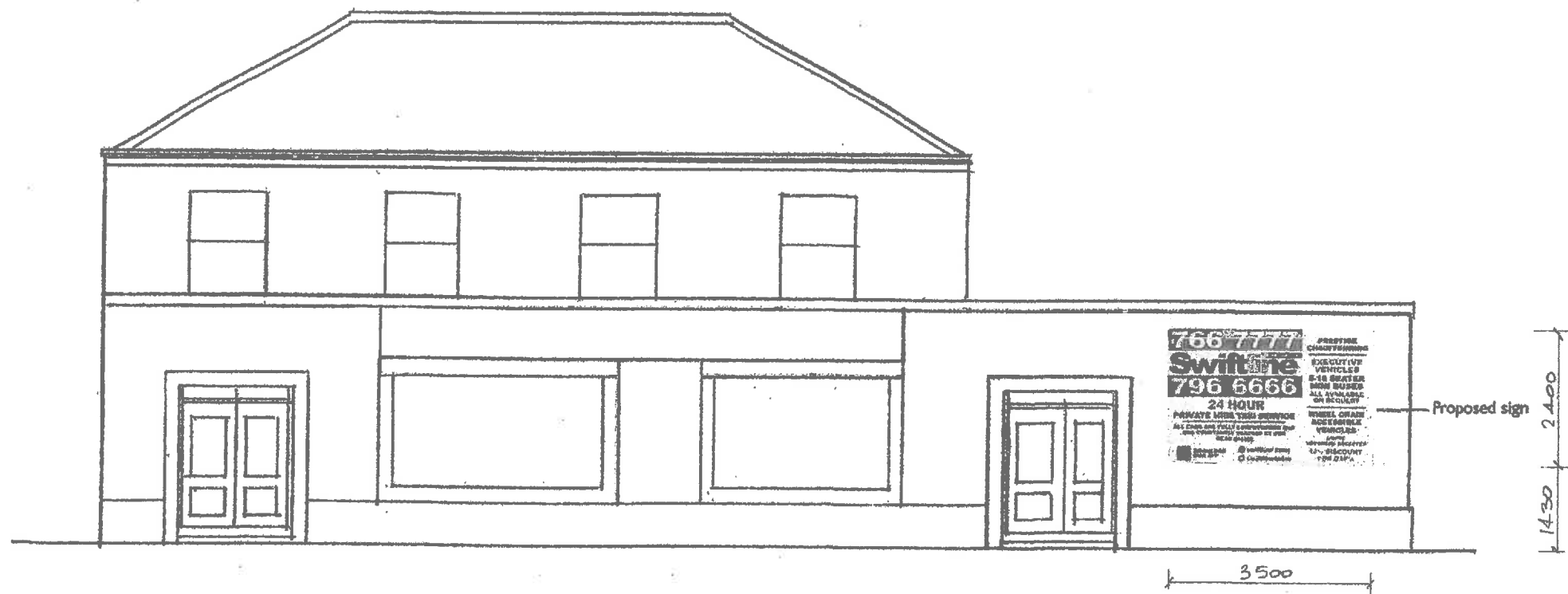
**(C) Crown Copyright and database right (2015). Ordnance Survey 100023063.**



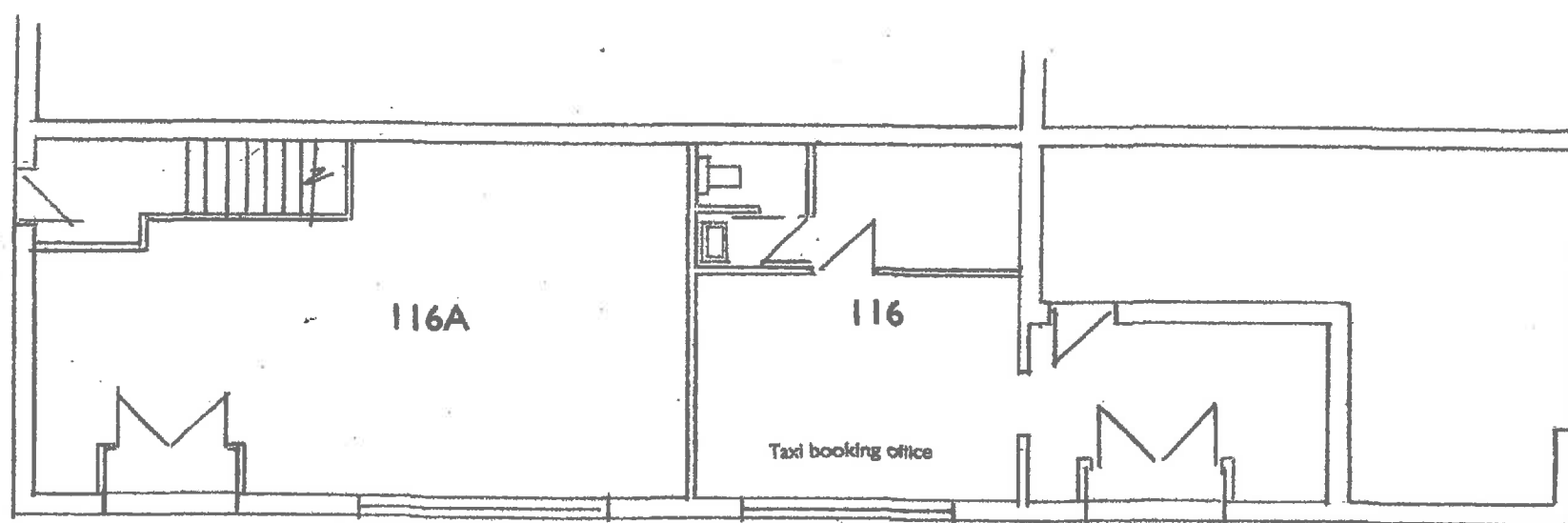
59051

Photo 1

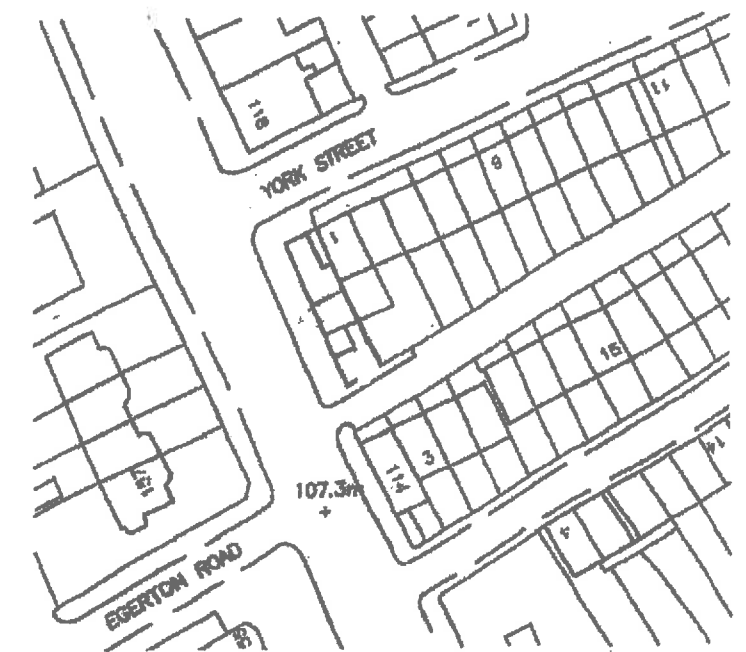




Proposed Existing Front Elevation



Proposed Ground Floor Plan



**Thompson Designs**  
 Tel 0161 705 1458 Mob 07884 318634  
 Sheet 2  
 Proposed Plans & Elevations  
 116 Bury New Road  
 Whitefield M45 6AD  
 Scale 1 : 100 Date July 2015

**Ward:** Whitefield + Unsworth - Pilkington Park

Item 06

**Applicant:** Mr & Mrs Pearson

**Location:** Land adjacent to 5 West Avenue, Whitefield, Manchester, M45 7SA

**Proposal:** Outline - Demolition of existing building and erection of 4 no. dwellings

**Application Ref:** 59058/Outline Planning  
Permission

**Target Date:** 28/09/2015

**Recommendation:** Approve with Conditions

### **Description**

The site is located off West Avenue and contains a single building, which was last used as a dairy. The site is overgrown and was last used as a garage colony. There are some mature trees on site and a 2 metre high paladin fence on the perimeter. Vehicular access is taken from the back street to the west, which then connects onto West Avenue.

There is a builders yard to the east and there are residential properties to all other boundaries.

The applicant seeks outline consent for the demolition of the existing building and the erection of 4 dwellings, including the layout and means of access. Two of the proposed dwellings (plots 1 & 2) would front onto West Avenue and two dwellings (plots 3 & 4) would be located near the rear of the site. Access would be taken from the back street to a centralised parking area.

### **Relevant Planning History**

56266 - Erection of 2 metre high mesh perimeter fencing and gates at land opposite 2 West Avenue, Whitefield. Approved with conditions - 3 July 2013.

### **Publicity**

42 neighbouring properties (14 - 32 Savoy Court; 1 - 5 West Avenue; 325, 327A Bury New Road; 42 - 62 Lily Hill Street) were notified by means of a letter on 6 August 2015.

2 letters have been received from the occupiers of 33 Savoy Court and 44 Lily Hill Street, which have raised the following issues:

- Do not object to the principle of residential development.
- Have concerns relating to its layout and loss of habitat.
- The site has been vacant since February 2013. Until then the land had been in constant use as a garage colony for over 40 years.
- The plans indicate access and egress from an unmade track, which also runs along the back of the properties on Lily Hill Street. It is in a poor condition.
- The developer should be made to reinstate this road as part of any planning approval.
- Access during construction would be better from West Avenue.
- Prefer that the trees are retained.
- Welcome the proposed development. Ask that the two trees in the south east corner are removed as the roots are undermining the adjacent garages.

The objectors have been notified of the Planning Control Committee meeting.

### **Consultations**

**Traffic Section** - No objections in principle. Further comments to be reported in the Supplementary Report.

**Drainage Section** - Comments to be reported in the Supplementary Report.

**Environmental Health - Contaminated Land** - No objections, subject to the inclusion of conditions relating to contaminated land.

**GM Ecology Unit** - Comments to be reported in the Supplementary Report.

### **Unitary Development Plan and Policies**

EC2/2	Employment Land and Premises
H1/2	Further Housing Development
H2/2	The Layout of New Residential Development
H2/1	The Form of New Residential Development
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/5	Crime Prevention
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/5	Waste Water Management
EN8	Woodland and Trees
EN8/2	Woodland and Tree Planting
HT2/4	Car Parking and New Development
HT4	New Development
HT5/1	Access For Those with Special Needs
SPD1	Open Space, Sport and Recreation Provision
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

### **Issues and Analysis**

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

**Principle (Housing)** - Following revocation of the North West Regional Strategy on 20th May 2013, there is no statutory housing target for Bury. Work is in progress on Bury's Local Plan - 'The Core Strategy', which will bring forward a new statutory housing target. It is currently proposed that the new housing target in the Local Plan should be set at 400 dwellings per annum, which is currently timetabled to be adopted in 2014.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable housing land.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The site is located within the urban area and in a predominantly residential area. As such, the proposed development would not conflict with the surrounding land uses and would be located in a sustainable location with good access to public transport and services. The site



contains a red brick building and as such, would be previously developed land. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policy H1/2 of the Bury Unitary Development Plan and the NPPF.

**Design and layout** - The proposed layout indicates that there would be two dwellings fronting onto West Avenue with two dwellings located at the rear of the site. The proposed dwellings would be two storeys in height, which would match the dwellings in the immediate locality. The proposed dwellings would have a large side or rear garden, which would provide an acceptable level of private amenity space. There would be space within the rear gardens for bin storage. Therefore, the proposed development would be acceptable and would be in accordance with Policies H2/1 and H2/2 of the Bury Unitary Development Plan.

**Impact upon residential amenity** - SPD6 provides guidance on aspect standards between residential properties and would be relevant in this case.

There would be 20 metres between plots 1 and 2 and Nos 2 and 4 West Avenue and 38 metres between the dwellings on West Avenue and plots 3 & 4. These would comply with the aspect standards in SPD6.

There would be between 13 metres and 18 metres between the existing dwellings on Lily Hill Street and the proposed dwellings, which would be acceptable. The proposed rear gardens would be a minimum of 7 metres in depth, which would comply with the standards.

Therefore, the proposed development would not have an adverse impact upon the amenity of the neighbouring properties.

**Bats** - A bat survey was submitted with the application and states that there is no evidence to suggest that the building is used by bats and the trees provide a feeding source for bats. The report concludes that there is little roosting potential for the building and replacement trees should be planted for any trees that are removed. Comments from GM Ecology Unit will be reported in the Supplementary Report. Therefore, the proposed development would not cause harm to a protected species and would be in accordance with Policies EN6 and EN6/3 of the Bury Unitary Development Plan.

**Highways issues** - The proposed development would be accessed from the street at the rear of Lily Hill Street and would connect to a single parking area for all the proposed dwellings. The Traffic Section has no objections in principle to the proposal and further comments will be reported in the Supplementary Report.

**Parking** - SPD11 states that the maximum number of parking spaces is 1.5 spaces per 3 bed dwelling, which equates to a maximum of 9 spaces for the proposed development.

The proposed development would provide 6 parking spaces in a central parking area. The site is located in a high access area with good access to public transport. As such, the level of parking provision is considered to be acceptable in this instance. Therefore, the proposed development would be in accordance with Policy HT2/4 of the Bury Unitary Development plan and SPD11.

### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

**Recommendation:** Approve with Conditions

## Conditions/ Reasons

1. Applications for approval of reserved matters must be made not later than:
  - the expiration of three years beginning with the date of the grant of outline planning permission; and
  - that the development to which the permission relates must be begun not later than the expiration of two years from the final approval of the reserved matters or, in the case of approval on different dates, the final approval of the last such matter to be approved.

Reason. Required to be imposed pursuant to Section 51 of the Planning and Compulsory Purchase Act 2004

2. Before the development is commenced, the applicant shall submit detailed plans and particulars to the Local Planning Authority, and obtain their approval under the Town and Country Planning Acts, of the following reserved matters; the Iscale, appearance and the landscaping of the site.

Reason. To ensure the satisfactory development of the site and because this application is in outline only.

3. This decision relates to drawings numbered A2429/SITE, A2429/05 and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

4. No development shall commence unless and until:-

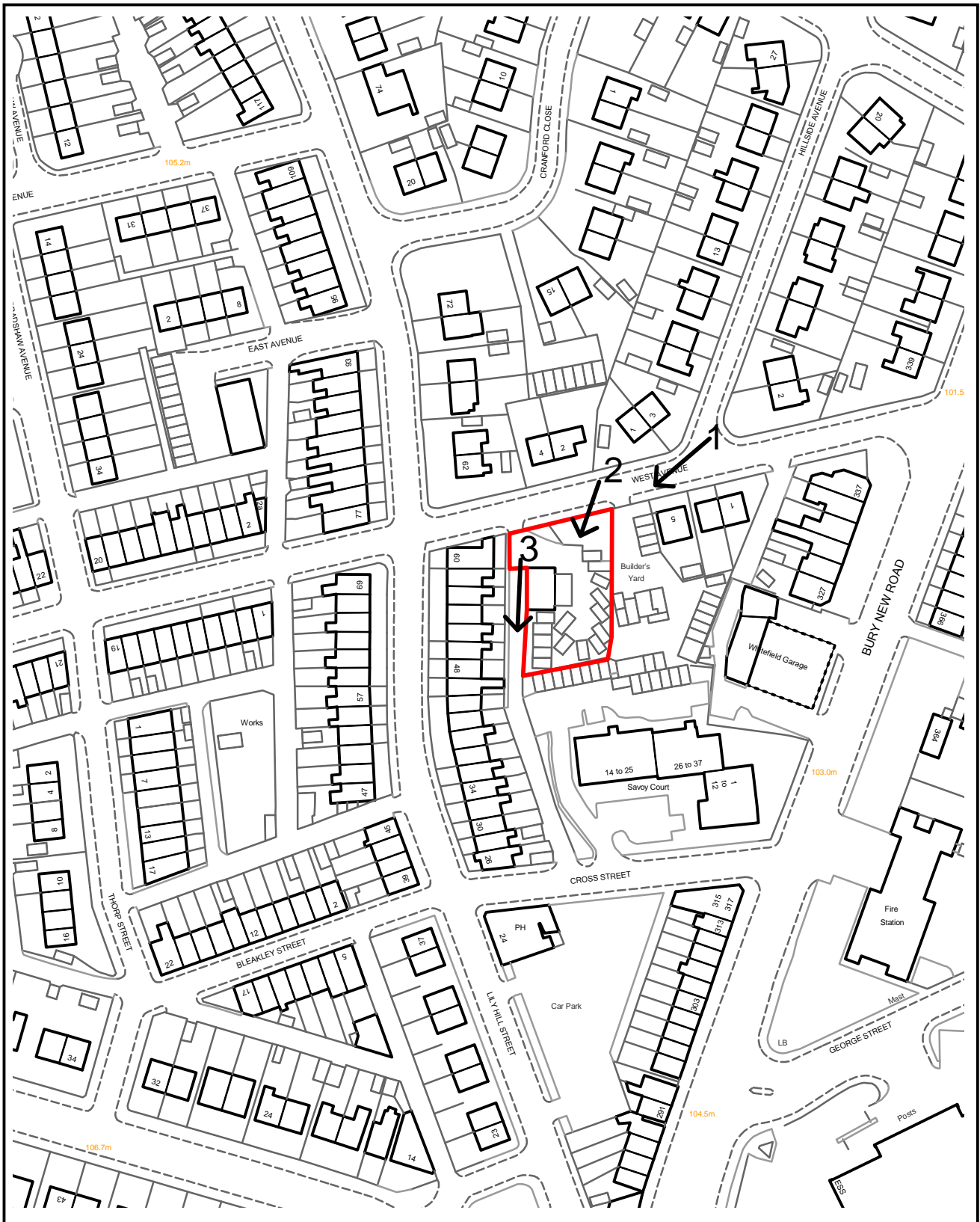
- A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
- Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
- Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

5. Following the provisions of Condition 4 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

# Viewpoints



## PLANNING APPLICATION LOCATION PLAN

**APP. NO 59058**

**ADDRESS: Land adj 5 West Avenue  
Whitefield**

**Planning, Environmental and Regulatory Services 1:1250**

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59058

Photo 1



Photo 2

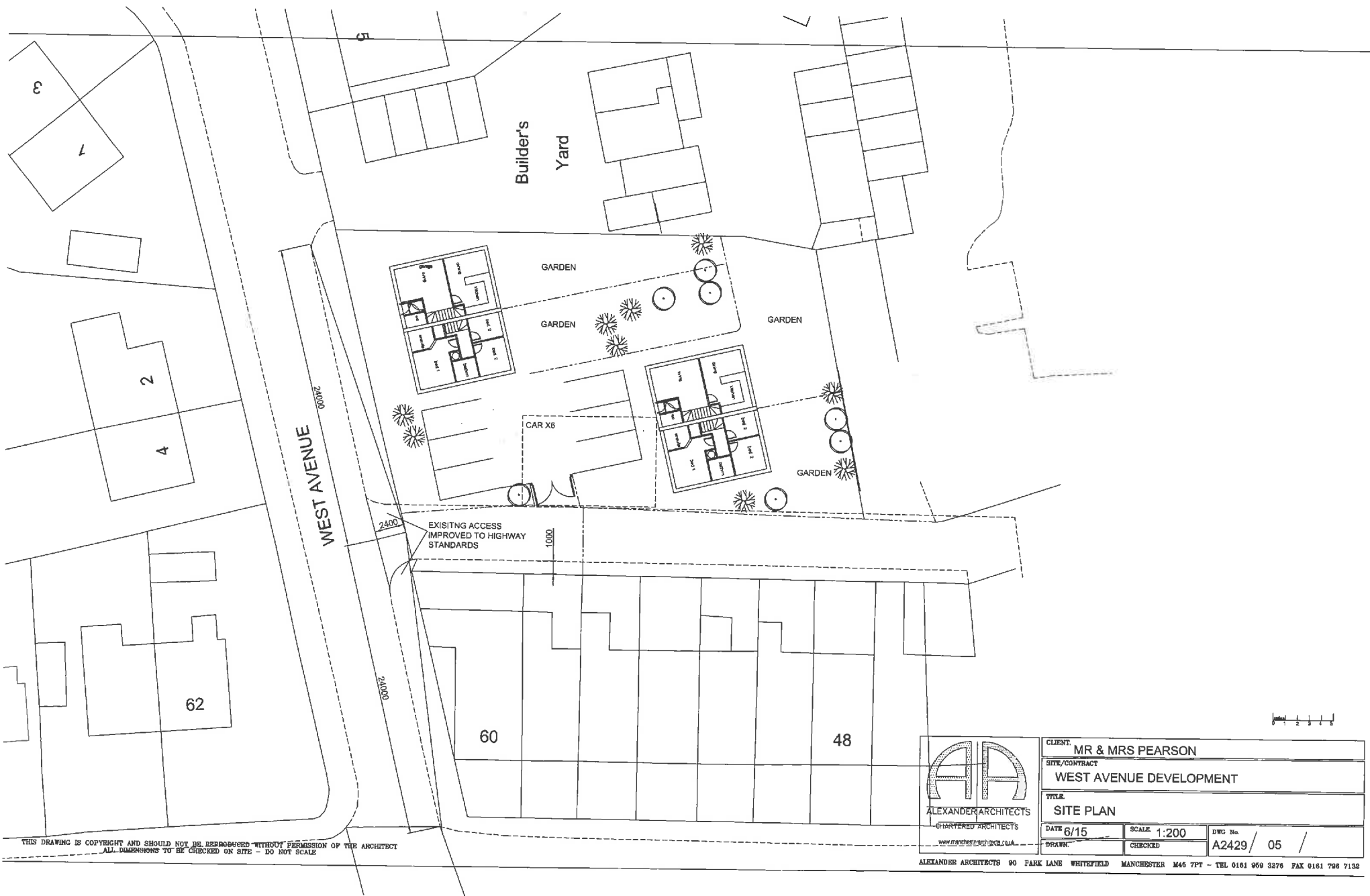




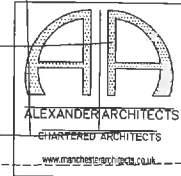
59058

Photo 3





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 ALL DIMENSIONS TO BE CHECKED ON SITE - DO NOT SCALE



CLIENT: MR & MRS PEARSON		
SITE/CONTRACT: WEST AVENUE DEVELOPMENT		
TITLE: SITE PLAN		
DATE: 6/15	SCALE: 1:200	DWG No.:
DRAWN:	CHECKED:	A2429 / 05 /